



Austin Road Business Park and Residential Community Master Plan

City of Manteca

Community Development Department

Adopted 2010
1st Amendment 2014

Austin Road Master Plan

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1. Introduction

A. Foreword

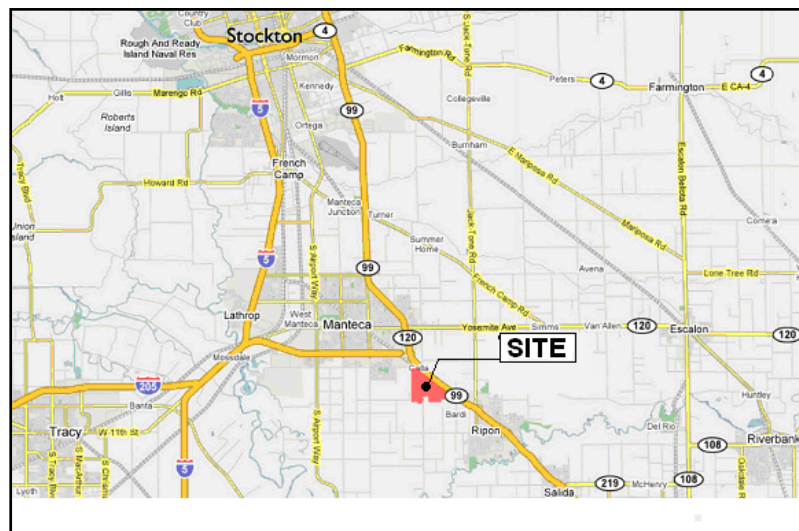
The Austin Road Business Park and Residential Community area comprises approximately 1,036 acres within the Primary Urban Service Boundary area of the City of Manteca's General Plan and is located at the southeastern area of the General Plan. The Austin Road Business Park and Residential Community is located at the intersection of Austin Road and State Highway 99 in San Joaquin County. The northwest border of the project site touches the southeast border of the City of Manteca. The proposed project would include heavy industrial, commercial, office, mixed use, various residential uses, and public/quasi-public uses. The project will include approximately 450 acres of residential development, 192 acres of commercial and retail development, and 312 acres of Business Professional and Industrial Park development. The project will include approximately 11 acres of neighborhood parks, 20 acres of dual-use park/basins, and 18 acres of dual use basin/ trail.

B. Existing Conditions

Figure 1-1: Project Location

The Austin Road Business and Residential Community is located at the intersection of Austin Road and State Highway 99 in San Joaquin County. The Northwest border of the site touches the southeast border of the City of Manteca.

The area is generally bounded by East Woodward Avenue to the north, Highway 99 to the east, and is bisected by the existing Austin Road, which runs north-south. **Figure 1-1** shows the current project location.



The existing circulation within the project site consists of rural roads, with the exception of Woodward Avenue and Austin Road. Existing infrastructure on the site is limited; the rural residences on the project site have individual wells and septic systems. A portion of the project site is located in the Manteca Unified School District and the remainder is located within the Ripon Unified School District.

C. Areas of Focus

The project applicant has developed the following objectives for the proposed project:

1. Provide a diversity of employment and housing types and opportunities.
2. Provide a pedestrian oriented neighborhood compatible with the Austin Road Business Park and Planned Community site, with pedestrian features that include safe, comfortable sidewalks and relatively direct routes to schools, parks, and commercial services.
3. Provide access to neighborhood parks.
4. Create a distinctive neighborhood identity through street landscaping, gateways, and traffic circles.
5. Provide opportunities for retail, commercial, industrial, and professional uses that contribute to the economic base of the City of Manteca.
6. Provide a major employment center identified in the 2023 General Plan that could reduce the home-to-work commute by Manteca residents.
7. Establish high quality development that provides a gateway and a distinctive landmark to the City of Manteca along Highway 99.

D. Purpose and Nature of the Master Plan

The purpose of this Master Plan, as developed by the City of Manteca, is to guide the future development of this planning area. In addition, the City of Manteca desires to promote compatibility between proposed industrial uses and proposed residential land uses. Accordingly, the Master Plan is being undertaken to ensure that future development of the land, as described, is accomplished in a comprehensive manner consistent with the framework of master planned roads, building areas, and open spaces and that these internal uses relate positively to present and future surrounding development.

E. Planning Considerations

1. Consistency with General Plan & Zoning

- The land uses in the Manteca General Plan are the official land uses for the City. As part of the annexation of the area by the City, the land uses proposed in this Master Plan will constitute the designated uses for the area. Development within the Master Plan area will also be required to comply with requirements of the Manteca Municipal Code.
 - a) **Memorandum of Understanding (MOU) City of Manteca and City of Ripon**
 - The MOU between the Cities of Manteca and Ripon was adopted in July 2008, as part of the cooperative exchange of planning land area in each cities' respective General Plan. (See Appendix 1)

- As part of the MOU both cities agreed to work together to establish a Policy Statement in each City's General Plan to provide for cooperative planning efforts along the common General Plan Boundary of both cities. In addition, both Cities agreed to include provisions for the extension and connection of the pedestrian and bikeway system through the two communities along the State Highway 99/Union Pacific Railroad corridor. The Austin Road Business Park and Residential Community Master Plan Area is the southeastern most area of the Manteca General Plan. At such time that development in the southeastern portion of the project area is proposed the City will work cooperatively with the City of Ripon to ensure the proposed development provides proper extension of both pedestrian and bikeway system as well as meets the cooperative planning policies agreed on by both cities.

2. CEQA Compliance

- An EIR for the project area is being prepared concurrently with the Master Plan. All development within the Master Plan area will be in compliance with the California Environmental Quality Act (CEQA).

2. Master Plan Components

A. Guiding Principles

The following guiding principles of the Master Plan have been established. They are to be used by developers and their consultants in the design of specific projects as well as by City staff in the review of these projects.

1. Land Use

- Create a balanced community with housing, recreation, shopping, community gathering, and employment.
- Develop Heavy Industrial uses along the southeast portion of the site.
- Develop Business/Industrial/Professional uses as a buffer zone between Heavy Industrial and Residential areas.
- Develop residential uses along the western portion of the site, including low density residential, medium density residential and high density residential uses.
- Develop Commercial mixed uses and general commercial uses along the northern portion of the site bordering the U P Rail Facility.

2. Visual/Design

- Encourage better design and site planning to enhance the character of the City.
- Provide flexibility in the application of development standards
- Improve communication and participation among developers, and the City early in the design process.

B. Plan Components

1. Land Use/Zoning Use Categories

A mix of planned land uses are proposed with the intent of creating a balanced community with housing, recreation, shopping, community gathering, and employment. The land uses consist of heavy industrial, small scale light industrial, community commercial and residential uses.

The Master Plan Zoning Map (see **Figure 2-1**) shows the primary design concepts for the Community. The map establishes the general locations and categories of land uses for the entire Community. It sets the land use framework for subsequent development review, and defines the boundary limits of the overall Project. In addition to establishing the basic allocation of land uses, the Master Plan Zoning Map establishes the primary circulation system. The Map is intended to allow flexibility for subsequent subdivision applications that will define the specific mix, configuration and character of uses in greater deal.

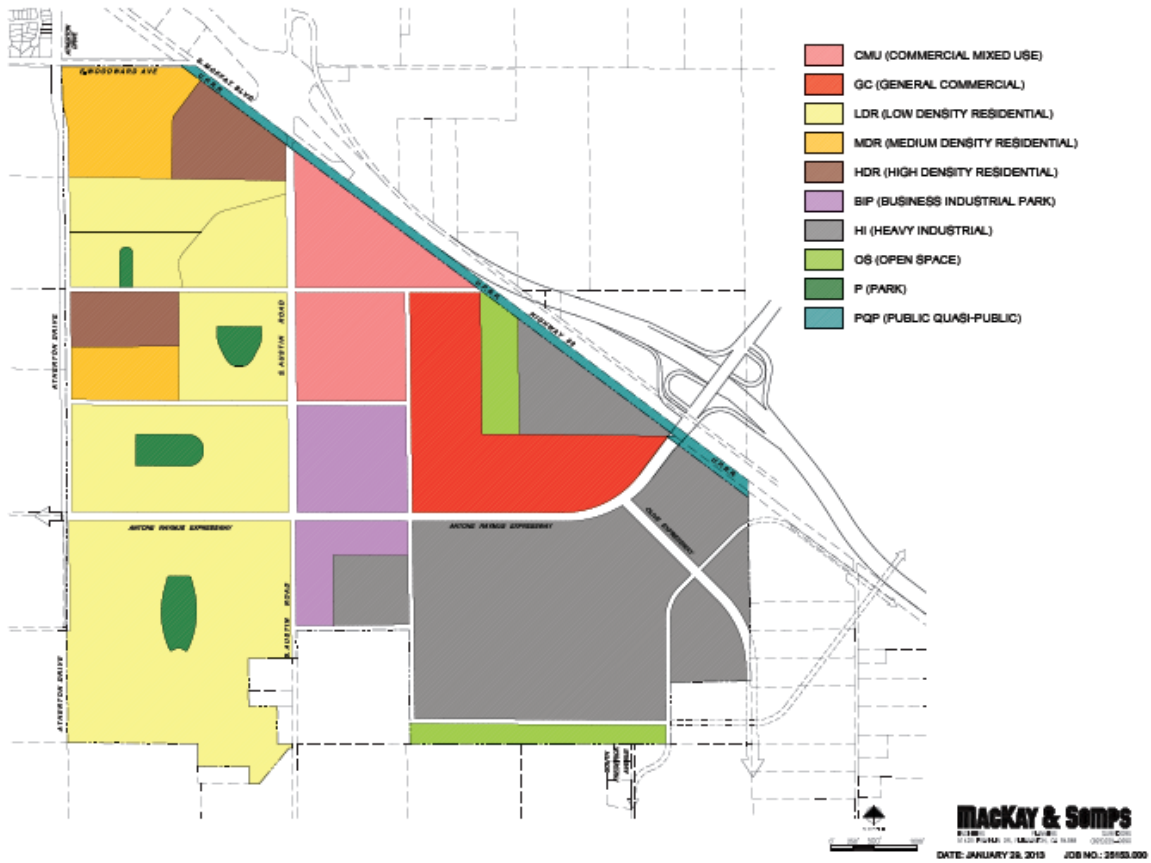
Uses under the General Plan include residential, community, and industrial uses. There are currently five properties at the northwest portion of the site under Williamson Act contracts.











Proposed zoning districts for the project site consist of General Commercial District (C-G), Community Commercial/ Commercial Mixed Use (C-C/ CMU), Business Industrial Park District (BIP), Heavy Industrial District (M-2), Single-Family Residential (R-1), Limited Multiple-Family Residential District (R-3), Multiple-Family Residential District (R-4), Public Quasi-Public District (PQP), Park (P) and Open Space (OS).










Figure 2-1 Master Plan Zoning Map

AUSTIN ROAD BUSINESS PARK & RESIDENTIAL COMMUNITY
MASTER PLAN

Revised 01.29.2013



- | | |
|--|--|
|  CMU (COMMERCIAL MIXED USE) |  BIP (BUSINESS INDUSTRIAL PARK) |
|  GC (GENERAL COMMERCIAL) |  HI (HEAVY INDUSTRIAL) |
|  LDR (LOW DENSITY RESIDENTIAL) |  OS (OPEN SPACE) |
|  MDR (MEDIUM DENSITY RESIDENTIAL) |  P (PARK) |
|  HDR (HIGH DENSITY RESIDENTIAL) |  PQP (PUBLIC QUASI-PUBLIC) |

	R-1 (SINGLE-FAMILY RESIDENTIAL)		
	R-3 (LIMITED MULTIPLE-FAMILY RESIDENTIAL)		C-C / CMU (COMMUNITY COMMERCIAL)
	R-4 (MULTIPLE-FAMILY RESIDENTIAL)		C-G (GENERAL COMMERCIAL)
	PQP (PUBLIC QUASI-PUBLIC)		BIP (BUSINESS INDUSTRIAL PARK)
	O-S (OPEN SPACE)		M-2 (HEAVY INDUSTRIAL)

2. Design Guidelines

Design guidelines apply to Heavy Industrial, Business/Professional, Commercial, residential, and public uses. The guidelines are to be used by applicants and their consultants in the formulation of specific development proposals. The guidelines will also be used by City of Manteca staff in the review of development proposals according to the terms and conditions of Section 17.49.040 Site Plan Review/Design Review of the Manteca Municipal Code.

The general objectives of the Master Plan Design Guidelines are to:

- Enhance a strong, Central Valley community while preserving its small town character.
- Create a sense of place at neighborhood and community levels.
- Create a safe and aesthetically pleasing environment for people to live, shop, work, and recreate.
- Create a live/work project that improves the community
- Employ sustainable design practices.

Prior to the submittal of development applications to the City for review, applicants shall show proof of design approval by the Austin Road Business Park and Residential Community Design Review Board (DRB). The DRB process shall be established by the Austin Road Business Park and Residential Community development team, and perspective proponents should confer with the DRB for compliance with their regulations and any applicable Covenants Codes & Restrictions (CC&Rs).

3. Phasing Plan

The phasing plan (See **Section 5 – Phasing Plan, Figure 5-1**) is developed to facilitate the business, commercial and industrial lands. There are five phases proposed. The first focuses on heavy industrial uses, the second focuses business, professional, and commercial uses, the third focuses on residential uses, the fourth focuses on general commercial uses, and the fifth focuses on heavy industrial and residential uses.

3. Land Use/Zoning Use Categories

A. Land Use

The proposed project includes a range of land uses and development intensities. The various land uses include heavy industrial, commercial, office, mixed use, various residential uses, and public/quasi-public. The public/quasi public uses include detention basins/parks, open space and an exposition (EXPO) center. The proposed EXPO center would include a 32,000-square-foot exposition hall for conferences; an amphitheater with 1,000 permanent seats and a 4,000-person-capacity lawn seating for outdoor events; and a 10,000-square-foot agricultural EXPO facility.

This Land Use Section provides a general summary of the land use categories within the planning area and the permitted uses and development standards for each land use designation.

1. Heavy Industrial (HI)

- This designation provides for manufacturing, processing, assembling, research, wholesale, and storage uses, trucking terminals, railroad and freight stations, and similar activities that require separation from residential uses due to noise, vibration or other characteristics incompatible with residential use.

2. Business/Industrial Park (BIP)

- The Business/Industrial Park designation is intended to provide sites for large uses in an office park environment that would include multi-tenant buildings. Typical uses permitted within the Business Industrial Park land use include:
 - administrative and general office
 - corporate or regional headquarters
 - research and development facilities
 - medical offices
 - professional services such as attorneys, accountants and insurance
 - light industrial, including manufacturing and assembly
- The business/industrial park land use will also permit a limited amount of service commercial and retail activities provided for the convenience of the employees within the area. The goal is to provide a mix of basic services in close proximity to employees to reduce daily convenience trips.

3. Commercial Mixed Use (CMU)

Example of a Commercial Mixed Use area

The Commercial Mixed Use designation will accommodate a variety of purposes including high density residential, employment centers, retail commercial and professional offices. The mixed use concept would integrate a mix of compatible uses on a single site that include sales, services and activities which residents



may need on a daily basis. With pedestrian access, these sites will enable residents to walk or bike for many local trips, instead of driving for convenience trips.

- CMU developments will include public facilities where feasible, such as a post office, library, fire station, or satellite government office.
- CMU developments may also include neighborhood work centers that provide space for private offices that use telecommunications services such that telecommuters and other residents in the neighborhood may work near their homes.

4. General Commercial (GC)

- The General Commercial category provides for wholesale, warehousing, and heavy commercial uses, highway oriented commercial retail, public and quasi-public uses, and similar and compatible uses. The designation is also intended to accommodate visitor commercial, lodging, commercial recreation and public gathering facilities, such as amphitheaters, or public gardens.

5. Low Density Residential (LDR)

Example of Low Density Residential

- The Low Density Residential land use will establish a mix of dwelling unit types and character determined by the individual site and market conditions. The density range allows substantial flexibility in selecting dwelling unit types



and parcel configurations to suit particular site conditions and housing needs. The types of dwelling units anticipated in this density range include small lots and clustered lots as well as conventional large lot detached residences.

6. Medium Density Residential (MDR)

- The medium density residential use includes single family homes and smaller scale multi-family developments, including garden apartments, townhouses, and cluster housing. The density range will accommodate small-lot single family homes that will typically be smaller in size and more affordable to residents.

7. High Density Residential (HDR)

Example of High Density Residential

- The high density residential use includes multi-family apartment style housing. The multi-family dwelling sites are typically located with direct access to arterial streets. The sites have access to the pedestrian and bikeway network along the street



corridor and are located along the conceptual route of a public transportation shuttle route. Most sites are near a neighborhood park or larger commercial facility.

8. Public/Quasi-Public (P/QP)

- This designation provides for government owned facilities, public and private schools, institutions, civic uses and public utilities, and quasi-public uses such as hospitals and churches.

B. Zoning

Figure 2-3 “Existing Manteca General Plan Designations” shows the current zoning status of the sight. Proposed zoning districts for the project site consist of General Commercial District (C-G), Community Commercial/ Commercial Mixed Use (C-C/ CMU), Business Industrial Park District (BIP), Heavy Industrial District (M-2), Single-Family Residential (R-1), Single-Family Residential Attached (R-2), Limited Multiple-Family Residential District (R-3), Multiple-Family Residential District (R-4), Open Space (O-S), and Public Quasi-Public District (PQP).

Table 3-2 below outlines permitted uses within the plan area. For those uses that are not listed, the Community Development Director shall make a determination of whether the proposed use is compatible with the desired zoning districts regulations. Additionally it would be the determination of the Community Development Director whether those uses not listed would be permitted, not permitted or conditionally permitted within the requested zoning district.

Table 3-1

AUSTIN ROAD BUSINESS PARK - COMMERCIAL AND INDUSTRIAL PERMITTED USES										
Use Type	R-1	R-2	R-3	R-4	C-C/CMU	C-G	BIP	M-2	PQP	O-S
RESIDENTIAL USES										
Single-Family dwelling detached	P	P	P							
Single-Family dwelling attached/duplex		P	P	P	C					
Multi-Family dwelling units			P	P	C					
Community Care Facility, Small	P	P	P	P	C					
COMMERCIAL USES - Offices	R-1	R-2	R-3	R-4	C-C/CMU	C-G	BIP	M-2	PQP	O-S
Business and Professional					P	P	P			
Financial institutions, including banks, savings and loans, or credit union					P	P	P			
Medical, dental, optical				C	P	P				
COMMERCIAL USES - General Retail Sales	R-1	R-2	R-3	R-4	C-C/CMU	C-G	BIP	M-2	PQP	O-S
Antiques					p	p				
Appliances					p	p				
Automobile, light truck, RV or garden implement, new and used sales, leasing, short term rental					C	P	C			
Heavy truck or farm implement new and used sales, leasing, short term rental					C	P	P			
COMMERCIAL USES - General Retail Sales	R-1	R-2	R-3	R-4	C-C/CMU	C-G	BIP	M-2	PQP	O-S
Garden implement, tires, parts and accessories					P	P	P			

COMMERCIAL USES - General Retail Sales (cont.)	R-1	R-2	R-3	R-4	C-C/CMU	C-G	BIP	M-2	PQP	O-S
Heavy truck or farm implement tires, parts and accessories						P	P			
Boat/ RV/ ATV/ Motorbikes Sales					C	P	P			
Retail building materials						P	C			
Christmas Tree, temporary					C	P				
Clothing/ Department Stores					P	P				
Drug and pharmaceuticals w/out Drive-thru					P	P				
Drug and pharmaceuticals w/Drive-thru					C	C				
Fabric, drapery, or upholstery					P	P				
Feed					C	P	C			
Furniture and appliances					P	P	C			
Gardening and landscaping supplies					P	P	C			
Gun					C	C				
Military surplus					C	P	C			
Pawn shop					C	C				
Pet store					P	P				
Pottery, ceramics, statuary					P	P	C			
Sporting goods and athletic equipment					P	P				
Tobacco					C	C				
Used clothing and household goods					C	C				
Adult Business					SU	SU				

COMMERCIAL USES - Food and Beverages Retail Sales	R-1	R-2	R-3	R-4	C-C/CMU	C-G	BIP	M-2	PQP	O-S
Bakery Retail					P	P				
Brewery, Micro, with food service					P	P	C			
Brewery, Micro						P	P			
Catering					C	P	P			
Convenience Market					C	P				
Liquor store					P	P				
Specialties, including meat, vegetables, health foods, or candy					P	P				
Supermarket					P	P				
COMMERCIAL USES - Eating and drinking establishments	R-1	R-2	R-3	R-4	C-C/CMU	C-G	BIP	M-2	PQP	O-S
Bar, tavern, or cocktail lounge					C	P				
Restaurant					P	P	C			
Restaurant or coffee with drive-thru					C	C	C			
COMMERCIAL USES - Services	R-1	R-2	R-3	R-4	C-C/CMU	C-G	BIP	M-2	PQP	O-S
Agricultural services						C	P			
Ambulance						C	P			
Appliance repair					C	C	P			
Artist studio					C	P	P			
Automobile body repair & painting, indoor Automobile repair and services						C	C			
Auto Gas Station, carwash					C	P	C			
Automobile towing							C			

COMMERCIAL USES – Services (cont.)	R-1	R-2	R-3	R-4	C-C/CMU	C-G	BIP	M-2	PQP	O-S
Barber, beauty shop, tanning, massage, day spa					P	P				
Carpet Cleaning, Furniture cleaning, refinishing, or upholstery						C	C			
Janitorial services							P			
Laundry, dry cleaning, pressing, and dyeing					C	C				
Laundry, dry cleaning, pressing, pick-up only, Laundromat, self-service					P	P				
Pest control						C	C			
Photography studio					P	P	P			
Publishing, Printing, cartography, lithography, or blue printing					C	P	P			
Recycling, collection facility, small					C	C	C			
Truck fueling station						P	C			
Veterinary out-patient clinic					C	P				
Veterinary hospital, Pet Grooming					C	P				
COMMERCIAL USES - Contracting services	R-1	R-2	R-3	R-4	C-C/CMU	C-G	BIP	M-2	PQP	O-S
General, building, heating and air-conditioning, plumbing, electrical, painting, engineering, carpentry, roofing, masonry, concrete, etc.							P			
Contractor or towing storage service yard							P	P		
INDUSTRIAL USES	R-1	R-2	R-3	R-4	C-C/CMU	C-G	BIP	M-2	PQP	O-S
Agricultural industries								P		

INDUSTRIAL USES (cont.)	R-1	R-2	R-3	R-4	C-C/CMU	C-G	BIP	M-2	PQP	O-S
Biomass energy conversion							C	P		
Brewery								C		
Cold storage facility								P		
Creamery								C		
Distillation for alcoholic beverages								P		
INDUSTRIAL USES - Industrial Manufacturing or Assembly	R-1	R-2	R-3	R-4	C-C/CMU	C-G	BIP	M-2	PQP	O-S
Acids, chemical, gases, etc.								C		
Aircraft, Automobiles, parts								P		
Appliances								P		
Building materials								P		
Cabinet shop						C	P	P		
Drug and pharmaceuticals							C	P		
Electronic Equipment (non-water using)						P	P	P		
Fertilizer								C		
Food products								P		
Textiles, furniture							C	P		
INDUSTRIAL USES - INDUSTRIAL STORAGE	R-1	R-2	R-3	R-4	C-C/CMU	C-G	BIP	M-2	PQP	O-S
Automobiles, Recreational vehicles, Truck and heavy equipment								P		
Bottled gas								C		
Chemical								C		

INDUSTRIAL USES - INDUSTRIAL STORAGE (cont.)	R-1	R-2	R-3	R-4	C-C/CMU	C-G	BIP	M-2	PQP	O-S
Cold storage								P		
Equipment and building material								P		
Feed								C		
Moving and mini-storage							P	C		
Petroleum or petroleum based fuels								C		
Warehouse and distribution							P	P		
INDUSTRIAL USES -Other Industrial Uses	R-1	R-2	R-3	R-4	C-C/CMU	C-G	BIP	M-2	PQP	O-S
Bakery, large							C	P		
Beverage bottling works								P		
Billboard sign fabrication and storage								P		
Laboratory, experimental							C	P		
Metal, glass or paper recycling facility						C	C	C		
Research and development							P	P		
TRANSPORTATION FACILITES	R-1	R-2	R-3	R-4	C-C/CMU	C-G	BIP	M-2	PQP	O-S
Automobile parking lot commercial					P	P	C			
Automobile parking garage, commercial					P	P	C			
Bus depot					C	P	C			
Heliport	C	C	C	C	C	C	C	C	C	C
Railroad depot, multi-modal station							C	C		
Taxi depot							C	C		
Truck and motor freight transfer, terminal							C	P		

UTILITY AND COMMUNICATION FACILITIES	R-1	R-2	R-3	R-4	C-C/CMU	C-G	BIP	M-2	PQP	O-S
Communications building						C	C	P	C	
Microwave relay station						C	C	P	C	
Public utility building and yards						P	P	P	C	
Radio, television, or commercial communications transmitter, receiver, or translator						C	C	P	C	C
Satellite antenna, residential	P	P	P	P						
Transmission lines, towers, poles, pipelines, and underground facilities for gas, water, electricity, telephone or telegraph service owned and operated by a public utility company	C	C	C	C	C	C	C	P	C	C
Utility substation						C	C	P	C	
RESOURCE EXTRACTION AND ENERGY DEVELOPMENT USES	R-1	R-2	R-3	R-4	C-C/CMU	C-G	BIP	M-2	PQP	O-S
Administrative offices, accessory structures and attendant equipment storage for natural resource extraction or processing uses						C	C	P		
Cogeneration facilities								C	C	
INSTITUTIONAL USES	R-1	R-2	R-3	R-4	C-C/CMU	C-G	BIP	M-2	PQP	O-S
Art gallery, Museum, public and non-profit					P	P			P	
Auditorium					C	P			C	
Cemetery, mausoleum, columbarium					C	C			C	C
Mortuary					P	P				
Child day care center	C	C	C	C	P	P	C			

INSTITUTIONAL USES	R-1	R-2	R-3	R-4	C-C/CMU	C-G	BIP	M-2	PQP	O-S
Church	P	P	P	P	C	C			C	
Club or lodge			C	C	P	P				
Community care facility, large				C	C					
Community care facility, senior	C		C	C	C					
Hospital				C	P	P				
Nursing home, Convalescent hospital				C	P	P				
Public agency or public utility buildings and facilities	P	P	P	P	P	P	P	P	P	P
EDUCATIONAL INSTITUTIONS AND SCHOOLS	R-1	R-2	R-3	R-4	C-C	C-G	BIP	M-2	PQP	O-S
College and university					C	C	C		P	
College and university extension					C	P	C		P	
Preschool				C	P	P			P	
Private school	C	C	C	C	C	C			P	
SPECIALIZED SCHOOLS	R-1	R-2	R-3	R-4	C-C	C-G	BIP	M-2	PQP	O-S
Arts, crafts, sports school					P	P			P	
Business or trade school					P	P			P	

4. Design Guidelines

A. Design Guidelines Overview

1. Overview

- Objectives

- Enhance a strong, Central Valley community while preserving its small town character.
- Establish a new southeast gateway to the City of Manteca.
- Create a sense of place at neighborhood and community levels.
- Create a safe and aesthetically pleasing environment for people to live, shop, work, and recreate.
- Create a live/work project that improves the community.
- To create a regional events center to benefit the City of Manteca.

- Purpose

Project Design Guidelines (PDG) are intended to inform project designers, builders, architects, decision makers, and the public of expectation and preferences for the qualities to be incorporated in the development of the project. Since the development of the project will occur in phases over several years, the PDG establishes a common framework to guide individual design and improvement plans. The guidelines are not intended to limit creativity. They allow for variation necessary to respond to individual site conditions and consistent quality throughout all phases of the project development.

- Community Identity

The community identity of the Austin Road Business Park and Residential Community will be established by various design elements within the public rights of way, building facades, roofs, edges of development, and landscaping within individual projects.

The goal of the PDG is to guide designs of these elements throughout all phases of development to help create a community theme of continuity, livability, and sense of place, both at neighborhood and community-wide levels.

- Community Design Themes

Community design themes create identifiable symbols for the residential community and businesses through the treatment of building façade and roof forms and materials, monument sign forms, wall and fencing designs, street furniture, and lighting fixture styles.

A sense of permanence should characterize the design of private and public domain design elements. Materials for roofs and facades, columns, fences and walls should borrow from Central California traditions.

Parks located within individual neighborhoods and throughout the community should help greet residents and visitors to the neighborhood and foster neighborhood gathering. Structures within parks should include gateway elements, picnic pavilions, shade structures, walls, terraces, and park fences. Hipped roofs, heavy timber and stone construction should characterize park structure design.

2. Design Review

Design Review works toward achieving a better urban environment through attention given to fundamental design principles. Design Review is intended to affect how new developments can contribute positively to both the Project and the City of Manteca.

Design Review has three principal objectives

- To encourage better design and site planning to enhance the character of the city;
- To provide flexibility in the application of development standards; and
- To improve communication and participation among developers, and the City early in the design process.

Designs submitted by builders are intended to go through a design review process.

Preliminary design plans should identify the significant site feature and supporting reasoning behind architecture and site plan proposed. Prospective development in the Austin Road Business Park and Residential Community is subject to Manteca Regulations for design review and approval prior to building permits. Prospective developers shall coordinate with the City for necessary entitlements.

B. Industrial/Business Professional Guidelines

1. Site Design

- Objectives
 - Ensure a consistent level of architecture and site design treatment through all phases of development.
 - Reduce the visual effect of large expanses of paving through tree planting and screening

- Enhance business and industrial areas with attractive streetscapes.
- Site Requirements and Setbacks
 - Minimum lot size: 20,000 square feet.
 - City of Manteca zoning designation for Business/Industrial Park District are as follows:
 - BIP: Business/Industrial Park District
 - M-1: Light Industrial District
 - M-2: Heavy Industrial District
 - Building Coverage, Height and Setbacks are summarized in **Table 4-1:**

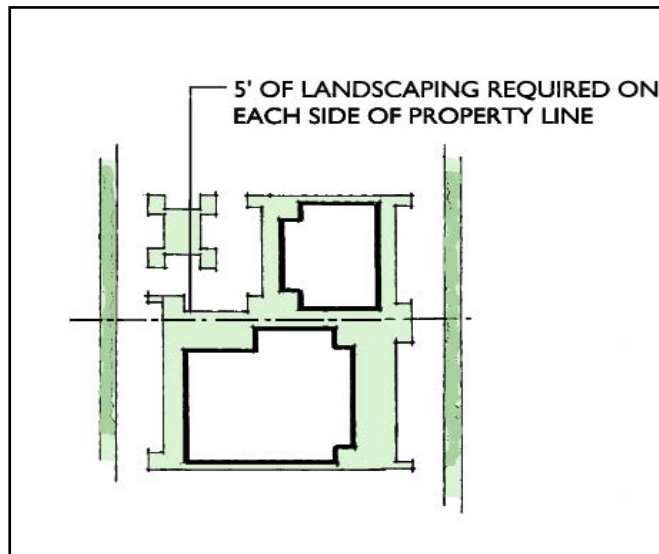
Table 4-1: Business/Industrial Building Coverage, Height, and Setbacks

Building Coverage, Height, and Setbacks						
	Front setback to building	Front setback to parking	Side setback to building	Rear setback to building	Max. building coverage	Max. building height
BIP (1 story)	40 ft.	15 ft.	20 ft.	10 ft.	40%	35 ft.*
BIP (2 story)	50 ft.	15 ft.	20 ft.	10 ft.	60%	35 ft.*
M-1 & M-2	25 ft.	15 ft.	10 ft.	25 ft.	N/A	N/A

* Any building proposed to exceed 35 feet in height may be approved provided the location, design, and materials will maintain the privacy, light, and air of nearby residential parcels and will be compatible with existing commercial and industrial buildings and parcels.

Figure 4-1: Plan depicting adjoining industrial properties

- A five (5) foot wide landscaped buffer is required adjacent to property lines in order to provide landscaping that gives variety to straight fence lines, large expanses of asphalt, and other hard surfaces, especially where these areas are visible from the public right-of-way. On industrial or warehouse and distribution sites this requirement will apply to property lines that are perpendicular to public streets. The landscaped buffer on these property lines



will be required to appoint 150 feet onto the parcel from the street right-of-way.

Figure 4-2: 20' Wide Landscape buffer along Union Pacific Railroad

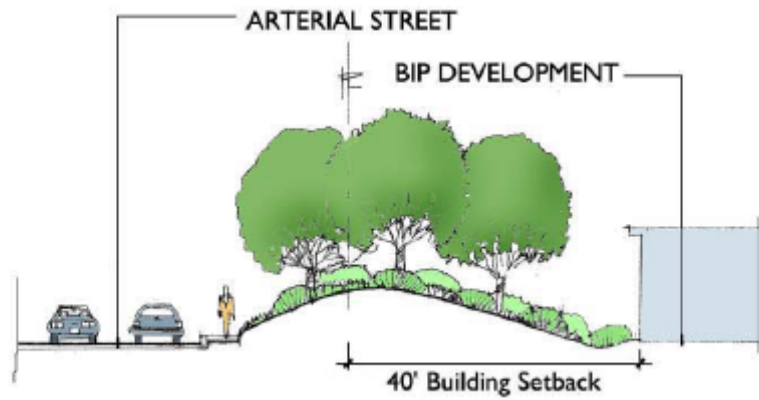
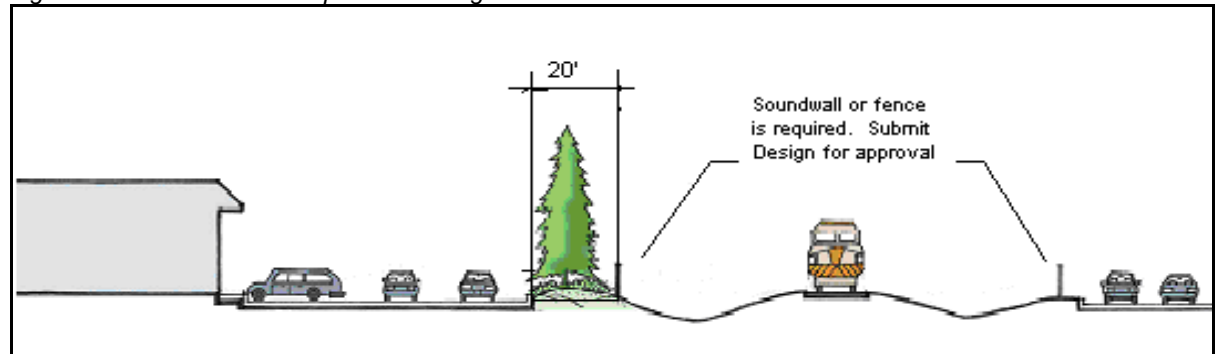


Figure 4-3 BIP Setback Without Parking (Arterial Street)

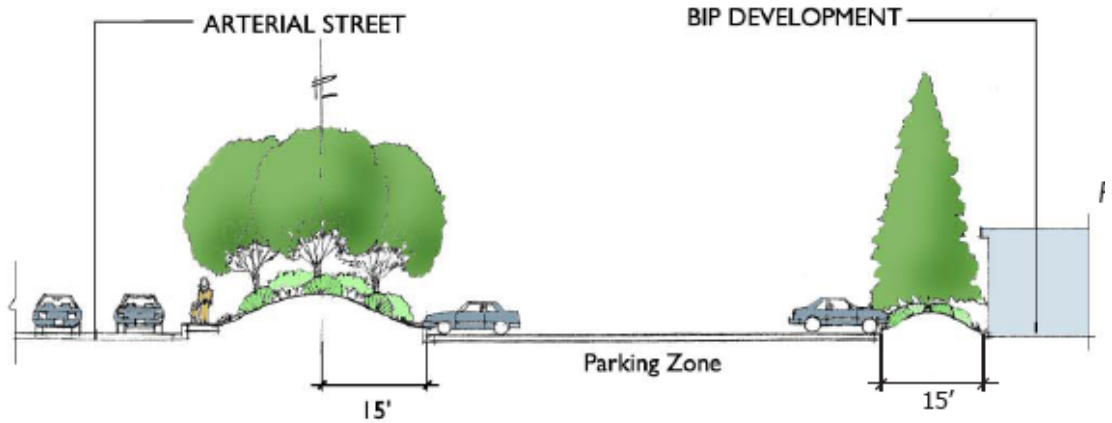


Figure 4-4 BIP Setback With Parking (Arterial Street)

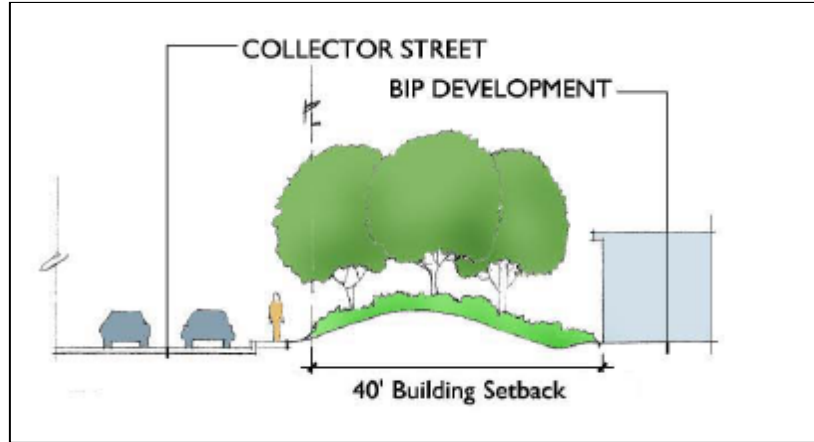


Figure 4-5 BIP Setback Without Parking (Collector Street)

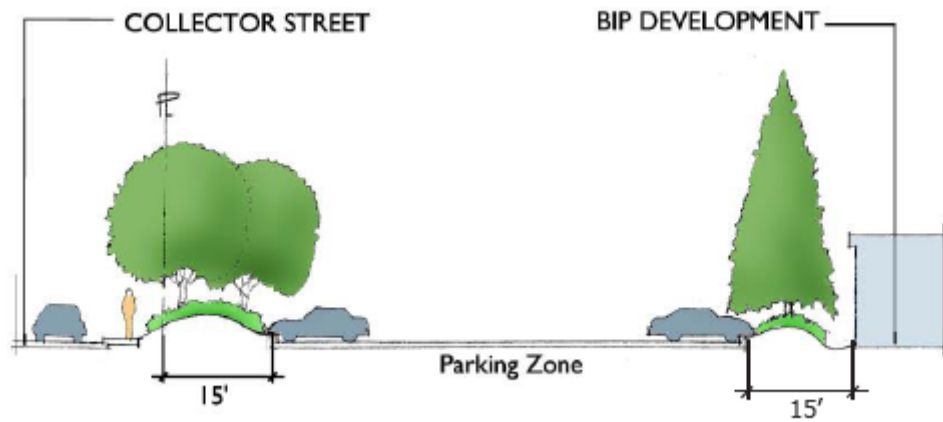


Figure 4-6 BIP Setback With Parking (Collector Street)

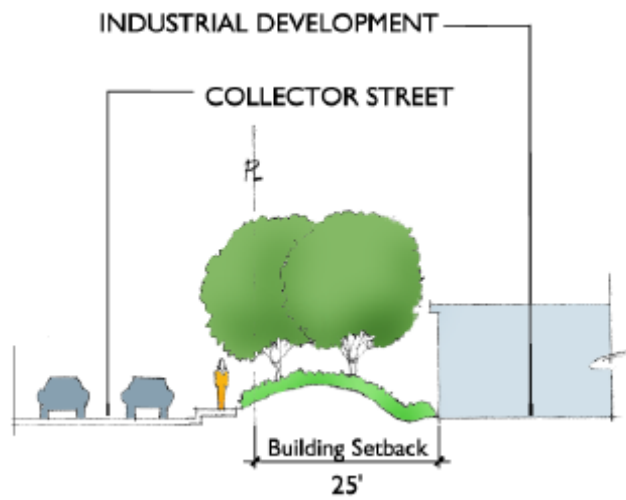


Figure 4-7 M1/M2 Setback Without Parking (Collector Street)

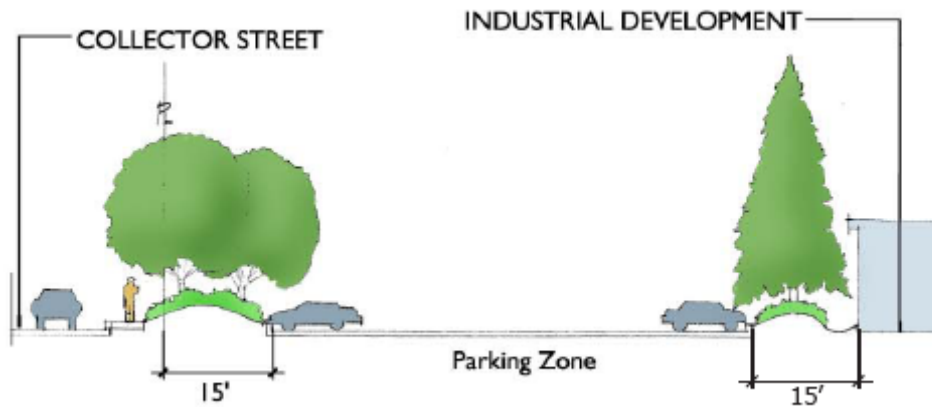


Figure 4-8 M1/M2 With Parking (Collector Street)

- **Circulation, Parking & Loading**

Parking shall not be permitted on any street or drive, or any place other than parking areas located on building sites.

Parking areas should be easily accessible from the street so that circulation to parking areas does not interfere with other site activities.

Visitor and handicapped parking should be located at the entrance of the building and be clearly marked.

Parking areas shall be screened from public streets by means of landscaping, berms or walls, solid evergreen shrubbery or fences. Minimum height of the screening shall be two (2) feet, six (6) inches.

A defined border or sidewalks shall be installed around all landscaped areas to contain and protect plant materials.

Full curb returns (as opposed to a standard driveway) shall be utilized for entries to all sites over ten (10) acres in size or for common driveways that serve two (2) adjacent sites that together total more than ten (10) acres.

Ganged driveways that

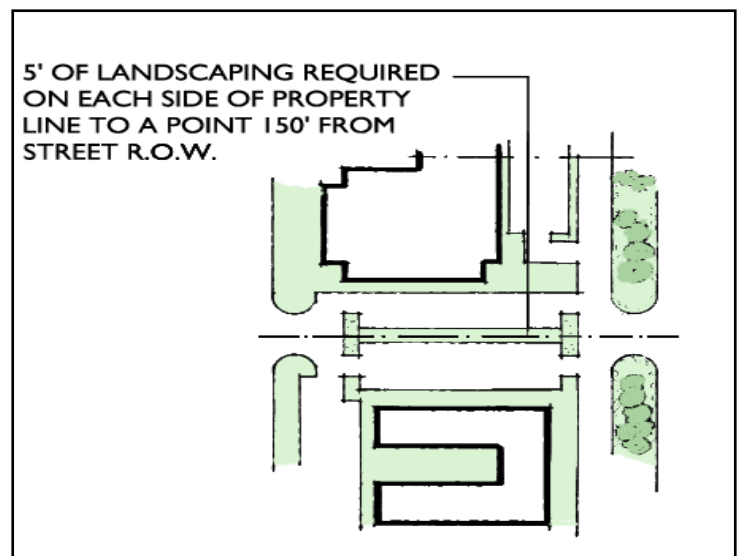


Figure 4-9: Plan Depicting 'Ganged Entry' concept
 Note: Rail Spur between two properties would preclude landscape buffer

serve two adjacent sites will be required to install landscaped islands along parking adjacent to the gang driveway and a landscape zone at the end of the common drive will act as a terminus to the view line down the ganged driveway. (See **Figure 4-9**)

No loading area shall be located within twenty-five (25) feet of residential property.

Truck or rail loading shall not be permitted between the building and the street, unless the buildings are set back from the curb a minimum of 125 feet and doors are screened by landscaped berms and/or fences.

All parking areas and drives shall be properly illuminated at the level of one foot candle or such greater level as may reasonably be required for areas subject to heavy night-time vehicular traffic. All parking areas shall be maintained for safe operation of vehicles and to present a slightly and well-kept appearance, and in conformance with §17.13.

- Landscape Design and Streetscape Design

- Landscape

On-site landscaping along arterials between the property line and the building, parking lot, or vehicular maneuvering or circulation improvements shall be installed and maintained by the property owner. The improvements shall be designed as an extension of the adjacent public arterial landscaping.

Perimeter landscaping is required adjacent to street frontages. These perimeter areas should include trees, shrubs, and ground cover. Landscaped berms are encouraged to soften the transition between street and parking lot.

Landscaping is required in the zone between office buildings and parking along street frontages. The total landscaped zone shall be a minimum of fifteen (15) feet measured from face of curb to face of building. Up to five (5) feet of this zone may be taken up by a sidewalk.

Completion of the landscaping on site shall be simultaneous with completion of the building and other improvements on the site.

As stated above, a mixture of trees, shrubs, and ground covers is required in any landscape area. Toward this end at least one (1) tree shall



be provided for each 2,000 square feet of landscaped area between buildings and street property lines. *Example of proper use of groundcover*

Trees must be a minimum of fifteen (15) gallon size at planting.

Reasonable access through landscaping shall be provided to public and private utility lines and easements for installation and repair.

Both perimeter and interior landscaping shall include canopy trees. The location and spacing of trees is dependent on the type of tree used, but the effect should be a consistent tree cover that will provide shade. Generally on office sites, a tree should be installed for every eight (8) parking spaces.

The plant palette should be relatively limited and applied in groupings of similar species rather than a few plants of many different species planted together.

The use of water conserving plants, such as California natives and drought tolerant trees, shrubs, and turf is encouraged.

Live plant materials should be used in all landscaped areas. The use of gravel, colored rock, bark, and other similar materials are not acceptable as a sole ground cover material.



Automatic irrigation is required for all landscaped areas. Plants should be watered and maintained on a regular basis. Irrigation systems should be designed so as not to overspray walks, buildings, parking areas, etc. The use of water conserving systems such as drip irrigation for shrub and tree planting is encouraged.

All undeveloped site areas should be seeded with perennial grasses prior to construction of the next phase of a project. All landscaped areas not leased for agriculture shall be mowed annually in the late spring, consistent with the City of Manteca nuisance abatement program.

- Streetscapes

The design of the streetscape should integrate, in a consistent and creative manner, plant materials, paths, landforms,

soundwalls, lighting, furniture, and signage to produce an attractive and functional environment.

Landscaping along arterials will be installed by the developer at the time curb and gutters are constructed abutting the property and will be maintained by the Property Owner's Association. Collector street landscaping will be implemented at the time of development of each lot along the collector. The Developer shall file a master landscape plan for the arterial setbacks and collector streets for review and approval prior to construction of said streets.

All landscaping should employ a mix of trees, shrubs, groundcovers, and turf where appropriate. The plant palette should be relatively limited and applied in groupings of similar species rather than a few plants of many different species planted together. The use of water conserving plantings, such as California natives and drought tolerant trees, shrubs, and turf is encouraged.

Live plant materials should be used in all landscaped areas. The use of gravel, colored rock, bark, and other similar materials are not acceptable as a sole ground cover material.

The use of lawn substitutes (such as rock with mortar, bomanite or shrubs) is encouraged in all medians and parkways. The use of turf should be minimized and reserved for areas of high use or visibility.

Automatic irrigation is required for all landscape areas. Plants should be watered and maintained on a regular basis. Irrigation systems should be designed so as not to overspray walks, buildings, parking areas, etc. The use of water conserving systems such as drip irrigation for shrub and tree planting is encouraged.

Figure 4-10: Streetscape Design Approach (1)

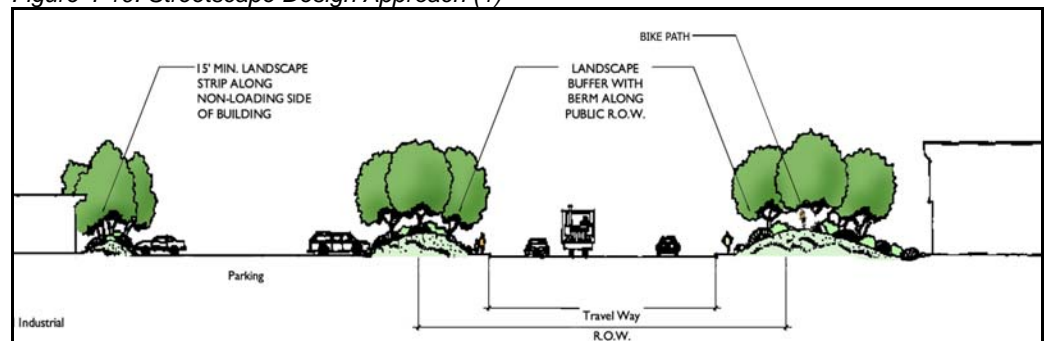
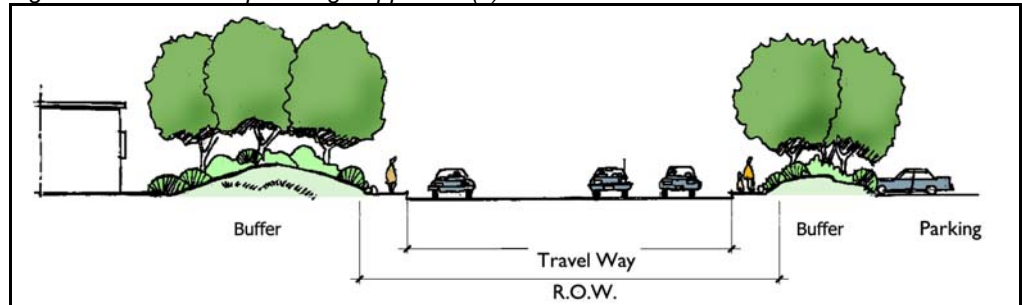


Figure 4-11: Streetscape Design Approach (2)



- Street Trees

Tree planting shall be symmetrical and of the same species, variety or cultivar in the parkways on both sides of the streets.

One tree variety or cultivar or pattern of no more than three varieties or cultivars shall be planted consistently at regular intervals along the entire length of a street. Spacing interval shall be no greater than forty (40') feet on center, where practical.

Where trees are planted in medians, the plantings shall be continuous and at regular intervals for visual balance and continuity. Spacing interval shall be no greater than forty (40') feet on center. Adequate sight lines shall be maintained at intersections. Please refer to Appendix A for a list of recommend trees for the Project Area.

Figure 4-12: Arterial Street Landscape Plan

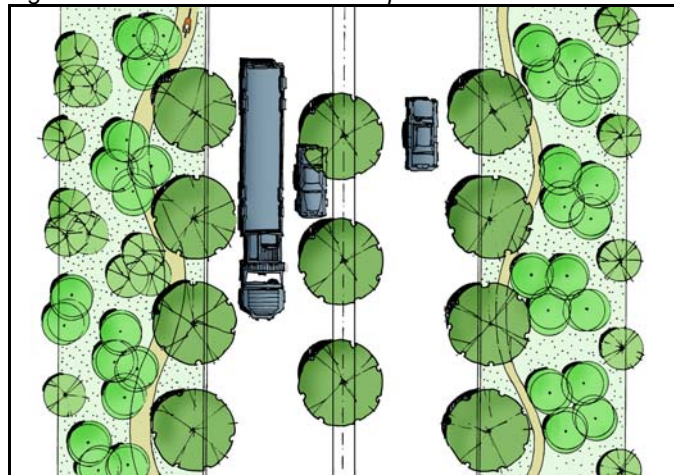
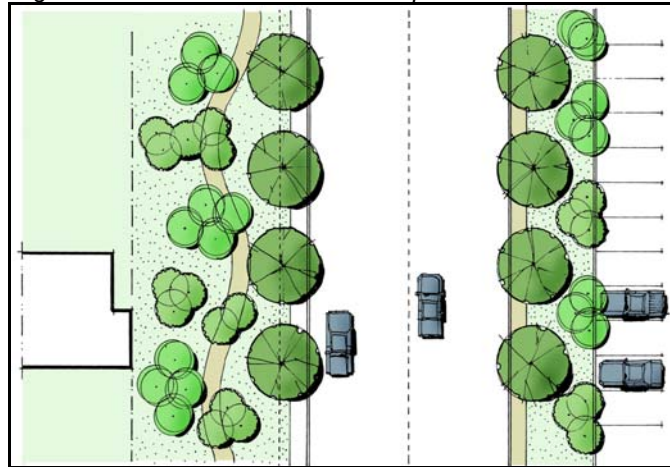


Figure 4-13: Collector Street Landscape Plan



- Fences, Walls, and Screening

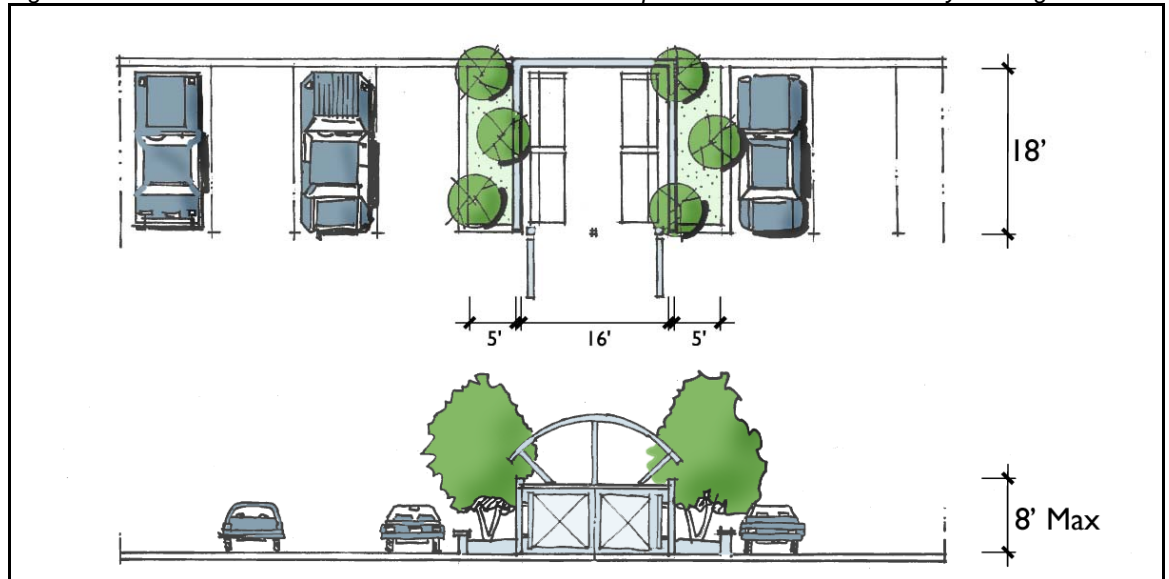
All exterior trash and storage structures and service areas should be screened from view with a solid wall or fence of a minimum height of six (6) feet above the street curb level. Storage areas shall be set back a minimum of fifty (50) feet from the streets unless fully enclosed in an architecturally compatible enclosure.

Soundwalls or fences used to buffer adjacent land uses shall be a minimum of six (6) feet in height.

No storage areas or fences are allowed within the landscape easement, front setback or side or rear yard landscape buffers.

Utility company equipment and roof-mounted equipment shall be screened from view.

Figure 4-14: Trash enclosures should be attractive and compliment architecture of nearby buildings



The design of masonry walls, fencing, trash enclosures, and similar accessory site elements should be compatible with the architecture of the building and should use similar materials.

Where masonry walls or fencing are used at property frontages, it is allowed only at the building line or the parking setback line. Walls and fencing should enhance the entrance to the property and shall not impair traffic safety by obscuring views.

Adequate fencing and/or walls shall be provided to guarantee preservation of privacy for adjacent residential uses.

Long expanses of fences or wall surfaces should be architecturally designed to prevent monotony.

All utilities between buildings and the streets within the project shall be placed underground.

Pad-mounted transformers, utility connections and meter boxes shall be screened and integrated into the site plan. Power lines of 35 kV or less must be placed underground.

Projects shall be designed to minimize noise impacts on neighboring noise-sensitive areas. Reducing noise levels can be achieved with a combination of dense landscaping, soundwalls, and additional setbacks where necessary.



Decorative masonry enhances this soundwall

2. Building Design

Architectural style of buildings should have a contemporary appearance and utilize elements that complement the existing character of Manteca. This means relating to the relatively small scale of adjacent structures and incorporating such elements as variation in textures and materials in the design of elements facing the public streets.

Metal framed buildings shall be allowed only to the extent that the industrial nature of the use mandates this type of construction. Such buildings shall be subject to design review to determine if metal buildings are appropriate, in which case surface treatment to the office portions of such structures facing the public street shall be required.

All sides of office buildings shall be attractively designed, using high quality materials. Any accessory buildings and enclosures, whether attached to or detached from the main building, shall be of similar compatible design and material.

Large buildings should have facades that include variations in form and texture. Continuous surface treatments of a single material should be minimized. In the event that this is done, textural changes or relief techniques are encouraged.



Building featuring a façade with varied form

Where an industrial area abuts a residential neighborhood, abrupt scale changes should not be allowed. The transition from residential to industrial should be gradual – starting with smaller, less intensive uses near the residential with the largest and most intensive uses farthest from the residential.

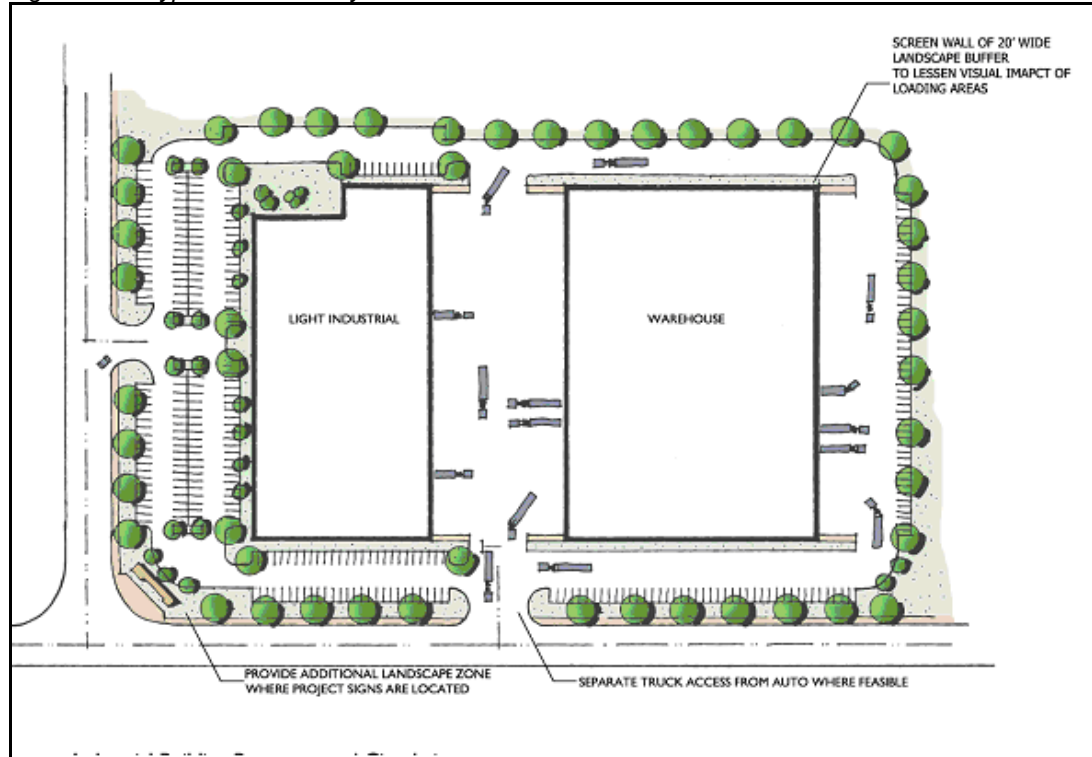
The primary building entry shall face the street and shall be clearly defined with special massing, detailing, lighting, and/or landscape treatment to make it stand apart from the rest of the building.



Building with attractive landscape features

Screen walls are encouraged for screening of utilities and loading areas.

Figure 4-15: Typical Industrial layout



3. Signs

All signage must be approved under the provisions of the adopted City of Manteca Sign Ordinance. Prior to the submittal of sign applications for City review, applicants shall show proof of design approval by the Austin Road Business Park and Residential Community Design Review Board (DRB). The DRB process shall be established by the Austin Road Business Park and Residential Community development team, and perspective proponents should confer with the DRB for compliance with their regulations and any applicable Covenants Codes & Restrictions (CC&Rs).

Monument sign that relates nicely to building materials

Signs shall be harmonious with the texture and color of the building.

At project gateways, the scale of the immediate intersection and surrounding area should dictate the scale of the gateway element.



A sign program shall be submitted as part of the application for a site.

A sign may be illuminated provided that no flashing, traveling, animated, or intermittent illumination shall be used. Such illumination shall be confined to the area of the sign except when such illumination is backlighting for an otherwise non-illuminated sign.

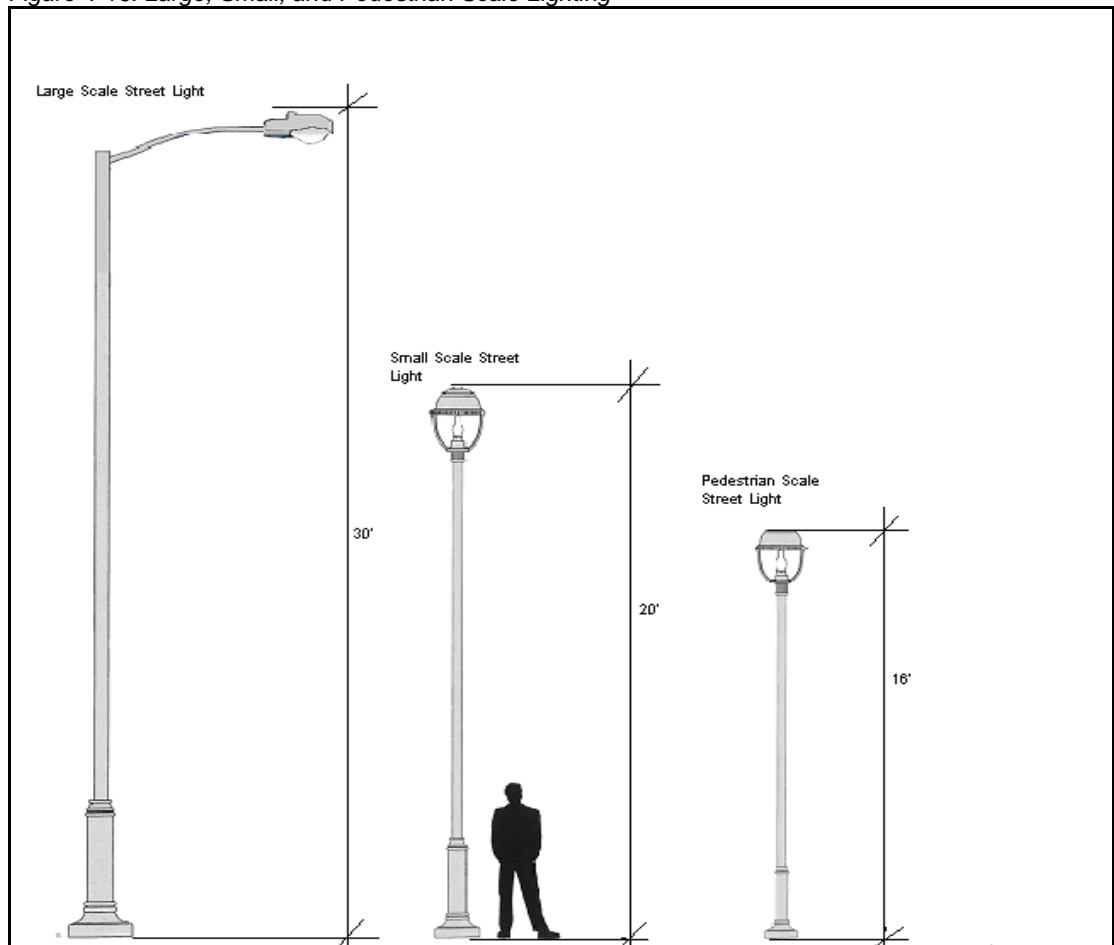
Monument signs shall be provided to identify the entries to large business complexes.

4. Lighting and Furniture

- Lighting

Lighting should be placed where it can best aid in illuminating activity areas. The site should not be overly lit. Electroliers should be scaled in size to match the size of the area to be lit.

Figure 4-16: Large, Small, and Pedestrian Scale Lighting



Area lighting should be directed downward and shall be placed to prevent glare or excessive spray of light on neighboring sites.

Accent illumination should be provided at key locations such as building entries and driveway entries. Pedestrian walkways, plazas, or other activity points shall be illuminated.

Lighting or highlighting of building facades is permitted but should be sensitive, subtle, and not excessive.

Parking and roadways should use either mercury vapor or high pressure sodium lamps.

Light fixtures and standards shall meet all safety standards and be employed throughout the length of each street. It is recommended that one lighting fixture style be employed for use on all streets.

- Furniture

Benches, bollards, trash receptacles, and other furnishings should be in appropriate locations, such as along pedestrian walkways and at bus stops and similar sites in the open space network.

All furnishings should be resistant to weather and vandalism



Bench



Bollard



Trash Receptacle



Bike Rack

Examples of proper use of furniture

Note: The images and photos within the **Lighting and Furniture** section show quality and character. Final specified site furniture and lighting shall be submitted at time of tentative map submittal.

C. Commercial and Retail Guidelines

1. Site Design

- Objectives
 - Ensure consistent level of architectural and site design treatment through all phases of development.
 - Encourage commercial development with pedestrian scale areas.
 - Announce southeastern gateway to the City of Manteca through the use of City’s name within pylon sign design requirements.
- Setbacks and Site Requirements
 - Minimum lot size: 10,000 square feet (Commercial General).
 - Building Coverage, Height, and Setbacks (See **Table 4-2**).

Table 4-2 Commercial Building Coverage, Height, and Minimum Setbacks

Commercial Building Coverage, Height, and Minimum Setbacks					
	Front Setback to Building	Side setback to building	Rear Setback to building	Max. building coverage	Max. building height
CG	10 ft.	10 ft.	10 ft.	35%	35 ft.*
CMU	10 ft.	10 ft.	10 ft.	35%	35 ft.*

* Any building proposed to exceed 35 feet in height may be approved provided the location, design, and materials will maintain the privacy, light, and air of nearby residential parcels and will be compatible with existing commercial and industrial buildings and parcels.

- Commercial structures abutting residential zoning districts over 30 feet in height shall be set back from the property line adjoining the residential zoning district an additional one foot for each two feet of building height over 30 feet.

▪ Circulation, Parking & Loading

Minimum Off-Street Parking Standards shall be consistent with §17.15 Parking and Loading, City of Manteca Zoning Ordinance, both for the amount and design of parking.

Parking areas should be easily accessible from the street so that circulation to parking areas does not interfere with other site activities.

Concrete curbs or sidewalks shall be installed around all landscaped areas to contain and protect plant materials.

No loading area shall be located within twenty-five (25) feet of residential property.

All parking areas and drives shall be properly illuminated at the level of one foot candle or such greater level as may reasonably be required for areas subject to heavy night-time vehicular traffic, and compliance with the Manteca Municipal Zoning Code.

All parking areas shall be maintained for safe operation of vehicles and to present a well-kept appearance.

Loading and delivery service areas should be located and designed to minimize their visibility, circulation conflicts, and adverse noise impacts to the maximum extent feasible. Loading and delivery service areas should be screened with portions of the building, architectural wing walls, freestanding walls, and landscape planting.

When commercial buildings abut residential properties, loading areas should be located at the side of the building away from residences whenever possible.

Parking lots should be designed with a clear hierarchy of circulation: major access drives with no direct access to parking spaces; major circulation drives with little or no parking; and parking aisles for direct access to parking.



Example of well-designed parking lot

Dead-end aisles, even with turnaround areas, are strongly discouraged and should be avoided if possible.

Compatible uses shall have connecting parking for ease of circulation without having to use residential streets.

- Pedestrian Circulation

Clearly defined pedestrian walkways or paths should be provided from parking areas to primary building entrances. Clear and convenient pedestrian access should be provided between the public sidewalk and the pedestrian areas of the project.



Parking areas should be designed so that pedestrians walk parallel to moving cars. Pedestrians should not be required to cross parking aisles and landscape islands to reach building entries.

Raised walkways, decorative paving, and bollards should be used to separate pedestrian paths from vehicular circulation areas to the maximum extent possible.



Use of bollards to separate pedestrian area from the street

- Landscape Design and Streetscape Design

- Landscapes

Landscaping should tie commercial development to the surrounding neighborhood. At major project entrances it should help define the center and provide a sense of arrival.

Medians should be landscaped with ornamental trees; annuals and/or flowering shrubs.



Landscape Materials frame this public space

On-site landscaping along arterials between the property line and the building, parking lot, or vehicular maneuvering or circulation improvements shall be installed and maintained by the property owner. The improvements shall be designed as an extension of the adjacent public arterial landscaping.



Completion of the landscaping on site shall be simultaneous with completion of the building and other improvements on the site.

Landscaping shall not obstruct sight lines at street or driveway intersections. A mixture of trees, shrubs, and ground covers is required in any landscape design. Trees must be a minimum of fifteen (15) gallon size at planting.

Reasonable access through landscaping shall be provided to public and private utility lines and easements for installation and repair.

Trees along street frontages shall be selected to match or complement street trees in the public right-of-way.

The plant palette should be relatively limited and applied in groupings of similar species rather than a few plants of many different species planted together. The use of water conserving plantings, such as California natives and drought tolerant trees and shrubs are encouraged. Please refer to Appendix A for a list of recommend trees for the Project Area.

Live plant materials should be used in all landscaped areas. The use of gravel, colored rock, bark, and other similar materials are not acceptable as a sole ground cover material.

Automatic irrigation is required for all landscaped areas. Plants should be watered and maintained on a regular basis. Irrigation systems should be designed so as not to overspray walks, buildings, parking areas, etc. The use of water conserving systems and bubbler irrigation for tree planting is encouraged.

All undeveloped site areas and building pads should be seeded with perennial grasses prior to construction of the next phase of a project.

A ten (10) foot landscaped berm or dense planting should be provided along all street frontages where parking is provided.

- Streetscapes

The design of the streetscape should integrate, in a consistent and creative manner, plant materials, paths, landforms, soundwalls, lighting, furniture, and signage to produce an attractive and functional environment throughout the length of the street.

All landscaping should employ a mix of trees, shrubs, groundcovers, and turf where appropriate. The plant palette should be limited to the designated palette for commercial areas. Plants should be massed in groupings of similar species rather than many different species together. The use of water conserving plants, such as California natives and drought tolerant trees, shrubs, and turf is encouraged.

Live plant materials should be used in all landscaped areas. The use of gravel, colored rock, bark, and other similar materials are not acceptable as a sole ground cover material.

The use of lawn substitutes is encouraged in all medians and parkways. The use of turf should be minimized and reserved for areas of high use or visibility.

Automatic irrigation is required for all landscape areas. Plants should be watered and maintained on a regular basis. Irrigation systems should be designed so as not to overspray walks, buildings, parking areas, etc. The use of water conserving systems such as drip irrigation for shrub and tree planting is encouraged.

- Street Trees

All streets located in commercial areas should be planted with street trees.

Street tree planting along commercial areas should communicate an ordered character by using large trees planted at regular intervals.

Commercial areas should also incorporate plantings of distinctive or accent trees to punctuate areas or entrances. Please refer to Appendix A for a list of recommend trees for the Project Area.

- Paving

Decorative paving should be incorporated at center entries, and in courtyards, plazas, and crosswalks.

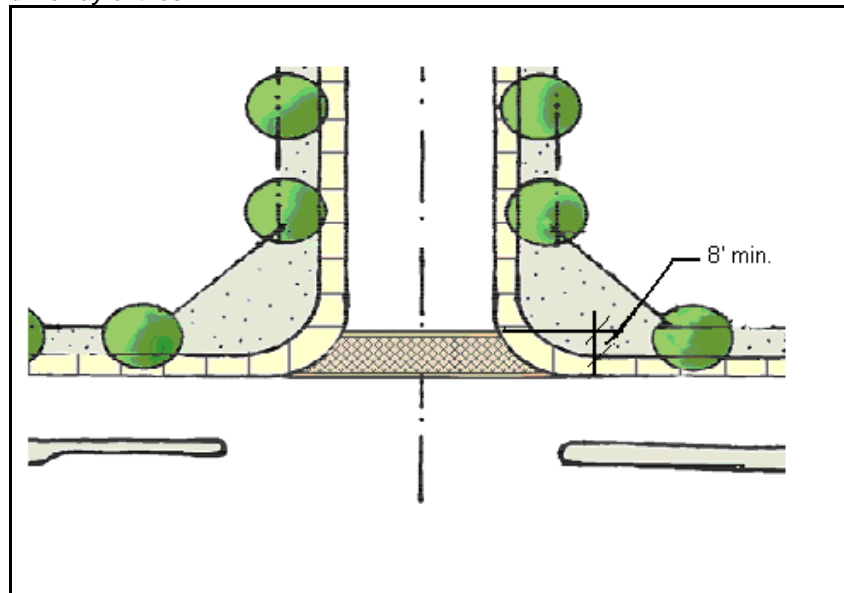
Paving materials should complement the architectural design of the building and landscape design of the development.



This pedestrian area features special paving

The use of stamped concrete, stone, brick, pavers, exposed aggregate, or colored concrete is highly encouraged.

Figure 4-17: Diagram showing minimum required depth for special paving areas at driveway entries.



The size of areas incorporating decorative paving should be consistent with the function of the area. At driveway entries, the minimum depth from the back of the sidewalk should be eight (8) feet; however, larger areas may be required.

- Fences, Walls, and Screenings

Trash enclosures should be designed with similar finishes, materials, and details as the primary buildings within the site. Trash enclosures shall have a solid gate.

Trash enclosures shall be separated from adjacent parking stalls with a minimum five (5) foot wide (interior clear dimension) planter and twelve (12) inch wide paved surface behind the curb.

Fences and walls should be constructed as low as possible, while still performing screening, noise attenuation, and security functions.

No screening wall or fence shall be located within a required landscape planter along the street frontage. No storage may exceed the height of the screening wall or fence.

Utility company equipment and roof mounted equipment shall be screened from street view. The design of masonry walls, fencing, trash enclosures, and similar accessory site elements should be compatible with the architecture.

Walls and fencing should enhance the entrance to the property and should not impair traffic safety by obscuring views.

Adequate fencing and/or walls shall be provided to guarantee preservation of privacy for adjacent residential uses.

Long expanses of fences or wall surfaces should be architecturally designed to prevent monotony.

All utilities between buildings and the streets within the site shall be placed underground

2. Building Design

- Building Architecture

Architectural style of buildings should have a contemporary appearance and utilize elements that complement the existing character of Manteca.

The construction and design of commercial buildings shall create a structure with equally attractive sides, rather than placing all emphasis on the front elevation of the structure.

Any accessory building or enclosures, whether attached or detached from the main building, shall be of similar compatible design and materials.

The incorporation of defined outdoor spaces into the buildings and site designs of all new development is encouraged. Outdoor spaces which are encouraged include courtyards, patios, plazas, covered walkways (arcades and colonnades), passages, gardens, trellised areas, etc.



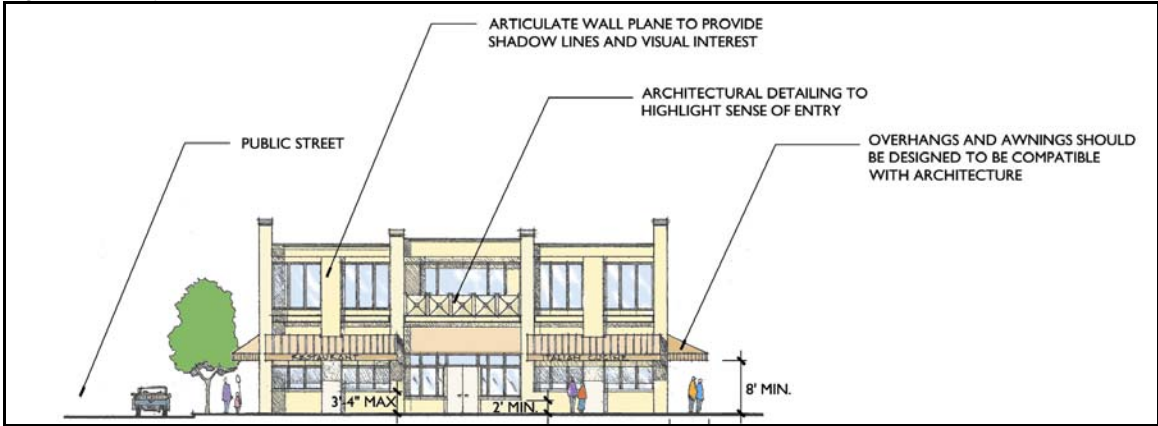
Photo depicting elements of Manteca's existing style

Long or continuous wall planes should be avoided, particularly in the Commercial Mixed Use area, where buildings should exhibit more detail and elements appropriate for close range pedestrian view.



Outside the Commercial Mixed Use District, building surfaces over two stories high or 50 feet in length should be relieved with changes of *Well-designed mixed-use retail architecture* wall plane that provide strong shadow or visual interest.

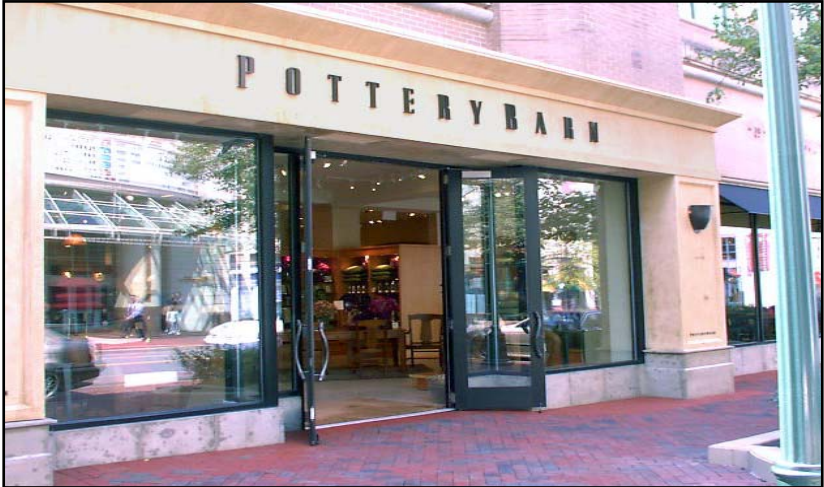
Figure 4-18: Typical Retail Elevation



- Storefronts

Storefronts should be comprised of transparent surfaces (windows). The use of clear glass (at least 80% light transmission) on the first floor is strongly encouraged.

Storefronts should promote a sense of entry into the structure as well as provide a sense of shelter by incorporating elements such as overhands, canopies, awnings, and recesses.



- Finish Materials

Exterior finish materials should be appropriate for an architectural style or theme of the building and should contribute towards a high quality image.

Changes in materials should occur at inside corners.

Materials should be varied to provide



Excellent use of storefront windows that highlight the building's entrance

architectural interest, however, the number of materials should be limited and not exceed what is required for contrast and accent of architectural features.

- Awnings

Awnings at both the ground level and upper floors should be designed to be compatible with the over all façade of the building and the window and door openings with which they are associated. The color of the awnings should be compatible with the color scheme of the building.

Awning placement should fit with the scale, proportion, and rhythm created by architectural elements. Awnings should not cover piers, pilasters, clerestory windows, or other architectural features.



These awnings are an appropriate scale and height

Awnings should be of high quality materials, shall be fire retardant, and be consistent with the overall building design. Aluminum, vinyl, or backlit awnings should not be used.

The minimum height of awnings should be eight (8) feet above the sidewalk and should not project more than six (6) feet out from the face of the building.

3. Signs

All signage must be approved under the provisions of the adopted City of Manteca Sign Ordinance. Prior to the submittal of sign applications for City review, applicants shall show proof of design approval by the Austin Road Business Park and Residential Community Design Review Board (DRB). The DRB process shall be established by the Austin Road Business Park and Residential Community development team, and perspective proponents should confer with the DRB for compliance with their regulations and any applicable Covenants Codes & Restrictions (CC&Rs).

Signs shall be harmonious with the texture and color of the building.

At project gateways, the scale of the immediate intersection and surrounding area should dictate the scale of the gateway element.

A sign program shall be submitted as part of the application for a site.

A sign may be illuminated provided that no flashing, traveling, animated, or intermittent illumination shall be used. Such illumination shall be confined to the area of the sign except when such illumination is backlighting for an otherwise non-illuminated sign.

Monument signs may be provided to identify the entries to large, contiguous shopping centers.

- Wall and Building Signs

Wall signs shall be located where the architectural features or details of the building suggest a location, size, or shape for the sign.

Individually-cut channel letters and numbers are strongly encouraged for internally lit signs.

Holes and disfigurations to walls left after signs are removed shall be patched and painted to match surrounding areas.

- Projecting Signs

The use of small, pedestrian-oriented signs is strongly encouraged for ground floor businesses and shops.

Supports and brackets for projecting signs shall be compatible with the design and scale of both the sign and the buildings.

Signs shall be hung at a 90 degree angle from the face of the building



and have at least eight (8) feet between the bottom of the sign and the sidewalk.

Projecting signs shall not be internally lit.

- Awning Signs

Text on awning signs shall only be located on the valence.

Lettering shall not exceed eight (8) inches. The color of lettering on awning signs shall be complimentary to the building's color scheme.

Temporary lettering and adhesive/press-on lettering are generally unacceptable.

- Freestanding Monument Signs and Highway-Oriented Signs

The design theme and material for the sign shall be compatible with the building or development advertised.

Pole signs are not allowed.

Freestanding highway-oriented signs shall be of a pylon-type design, supported by two or more appropriately dimensioned columns, not to exceed height and size requirements outlined in the Manteca Municipal Zoning Code.

4. Lighting and Furniture

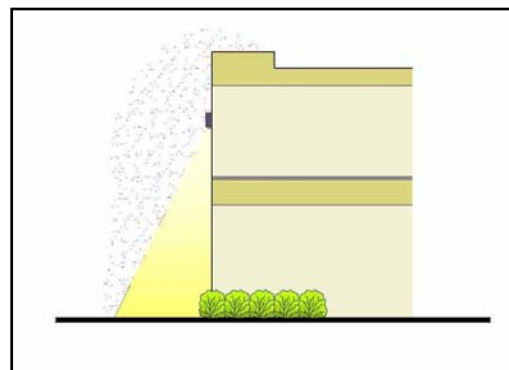
- Lighting

Lighting should be placed where it can best aid in illuminating activity areas. The site should not be overly lit. Electroliers should be scaled in size to match the size of areas to be lit.

Area lighting should be directed predominantly downward and shall be placed to prevent glare or excessive spray of light on neighboring sites.

Accent illumination should be provided at key locations such as building entries and driveway entries.

Pedestrian walkways, plazas, or other activity points should be illuminated.



Lighting or highlighting of building facades is permitted but should be sensitive, subtle, and not excessive.

Metal halide lamps should be used along roadways and in parking areas.



Large scale street lighting



Pedestrian level lighting

- Furniture

Site furniture should be placed at key locations such as building entries and along pedestrian paths.

The area surrounding site furniture such as bike racks and benches should be well illuminated.

Site furniture should be made of high quality, vandalism-proof materials and should be coated with a matte or gloss black powder coat finish.

NOTE: Images and photos within the **Lighting and Furniture** section show quality and character. Final specified site furniture and lighting shall be submitted at time of tentative map submittal.

D. Residential Guidelines

1. Overview

- Objectives

- Encourage attractive residential areas with distinctive character
- Provide a variety of housing types, densities, and lot size

- Definitions

- R-1: Single-Family Residential District

The primary purpose of the R-1 district is to provide for the development, protection and stability of single-family detached housing and compatible uses within the low-density residential neighborhoods of the city.



Attention shall be given to development in the R-1 district to ensure adequate light, air, privacy and open space for individual housing, and to provide space for community facilities and institutions, which are complementary with a low density residential environment.

○ R-2 Single-Family Residential Attached District

The primary purpose of the R-2 district is to encourage more affordable, innovative single-family attached and detached housing and compatible uses. Neighborhood amenities, property ownership and development standards are the same as the R-1 district, except that the lot width requirements have been modified to provide incentive for development of this type of dwelling without reducing the quality of housing in Manteca.

○ R-3: Limited Multiple-Family Residential District

The primary purpose of the R-3 district is to protect existing medium-density multiple-family housing, and to encourage the development of new



medium-density multiple-family housing and compatible uses. The density provisions of the R-3 district are intended to ensure adequate lots are used for dwelling units of varying sizes and to provide useable open spaces for families with children as well as for couples and single persons.

○ R-4: Multiple Family Residential District

The primary purpose of the R-4 district is to protect existing medium-density multiple-family housing and compatible uses. The R-4 district is intended to provide a complete and attractive environment for those persons who desire apartment and condominium living.

2. Setbacks and Site Requirements

▪ Setbacks

All dimensional development standards necessary to regulate housing development within the project area shall comply with the Manteca Municipal Zoning Ordinance. This information is supplemented with more detailed development guidelines in the narrative and illustrations in the sections that follow.

- Site Requirements

The Residential Development Guidelines provide the design framework for the Project and will ensure consistent and quality house and neighborhood design. The Design Guidelines address a wide variety of site and architectural elements, including lot configurations, driveways, garages, entries, and building facades, color schemes, massing, materials, and architectural details. Design review will be conducted for each building phase and for each product type by the Community Development Department (CDD).

3. Development Guidelines: Single Family Dwelling

- Site Design

In areas where there are changes in land use or residential density, new residential Development shall be designed to provide a transition between uses (through the use of setbacks, site plan, building massing, driveway locations, etc.).

Lot shapes generally should be simple and rectilinear.

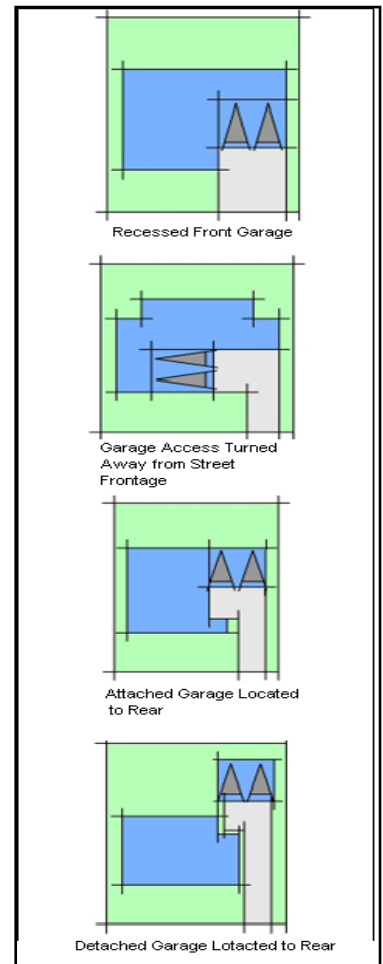
- Driveways

Driveways that are designed to serve more than two cars in width should incorporate alternative treatment including concrete finishes (salt or broom finishes), concrete banding, pavers, colored concrete, aggregate or brick banding, or other acceptable design elements.

In order to minimize the visual impact driveways have on the street scene, the following design concepts are encouraged where applicable:

- Side-entry driveways allow for the turning of the garage door away from the street
- Single-car width driveways that widen to two-car aprons at recessed or detached garages are encouraged. “Hollywood” driveways that permit turf or other low groundcover to be planted within the center of the driveway are encouraged on long driveways.

Figure 4-19: driveway configurations



- Different paving treatments to driveways, such as colored concrete, salt, finish, stamped concrete, pavers, insets, or other enhanced driveway material are encouraged.
 - Building Design
 - Entries

The entry of a residential dwelling shall be articulated as a focal point of the building's front elevation. Architectural features that enhance the entryway include: archways, columns, courtyards, porches, recesses or projections, roof elements, and windows.
 - Garage

Garages and driveways should not be visually dominant or the focus of attention of the unit. Minimizing the visual impact of a garage contributes to a visually interesting and more pedestrian-friendly environment.

Garage plans may vary greatly. Acceptable garage forms include front-loaded, rear-loaded, detached, swing-in, shallow-recessed, mid-recessed, deep-recessed, alley-loaded, and split garages.

Garage doors shall be recessed a minimum of 12 inches behind the garage wall plane.

Front loaded garage elements shall not exceed more than 50% of the overall width of the residence in 60' x 100' lots or larger.

Garage doors shall roll-up and not tilt-up.

Cut-in windows or other stylized improvements to the garage door façade to help reduce its mass and impact shall be encouraged.

Special paving accents shall be provided where alleys meet public right of ways. Garage drive aprons shall be a maximum of 5' in depth. Alleys shall be privately owned and maintained.

Porte-cocheres provide access to front-facing garages while minimizing the visual impact of the garage door. Porte-cocheres provide additional covered parking and can serve as an outdoor private space when not in use as a parking space.

Porte-cocheres are appropriately used with mid-recessed, deep-recessed, or detached garages. The height of porte-cochere, without living space above, shall not exceed 18'. Porte-cocheres without living space above shall be setback at least 3' from the side property line.

o Courtyards

Courtyards function as a transition from the public space of the street to the entrance of the dwelling and private outdoor living space.

Courtyards shall be located at the front entrance or adjacent to the primary living space of the residence.

Courtyards are encouraged where appropriate to the architectural style of the residence.

Entry courtyard creates transition from street to house



Freestanding courtyard walls, when provided, shall be finished to match the residence.

Stone ceramic tiles, steps, recesses, cutouts, wrought iron accents, or other elements appropriate to the architectural style are encouraged.

Courtyards shall be a minimum of 100 square feet with a minimum dimension of 10' in one direction.



Example of enclosed courtyard

o Façade

Where a single house design is used repeatedly, materials and detailing of major façade elements shall be varied.

Building facades should be articulated by using color, arrangement, or change in materials to emphasize the façade elements.

The planes of the exterior walls may be varied in height, depth, or direction.

Design elements and detailing shall be continued on facades that face streets. Such design elements shall include window treatments, trim detailing, and exterior wall materials.

Columns and archways shall be scaled appropriately to provide a sense of strength and support compatible with the architectural styling. They shall be along the front of the house, facing the street.

Porte-cocheres are encouraged where consistent with the architectural style of the residence.

Special design attention shall be directed to two-story facades.

Variation shall be provided to avoid visual monotony on long, straight portions of the street through the manipulation of the building elements and massing.

New housing development should avoid front elevations which mainly consist of rows of garage doors.

Windows and doors that face streets shall be detailed to add visual interest to the façade unless such treatment would be incompatible with the architectural style of the building.

- Windows

Principal window treatments are enhanced windows that create a focal point on the residence. Window treatments add architectural detailing and contribute to the residence's character and architectural style.

Where appropriate to the architectural styling, front elevations shall feature at least one principal window treatment and rear/side elevations that are visible from side streets shall have at least one



principal window treatment.

The use of shutters is an acceptable principal window treatment on visible rear/side elevations when used in conjunction with an enhanced sill or other form of articulation. Principal windows are defined as having one of the following characteristics: recessed window or pop-out surround; an enhanced sill with corresponding roof elements and corbels; decorative shutters; bay window with projection and detailing appropriate to the architectural style of the residence; or an overhead trellis element.

To complement the principal window treatment, all other windows on the front elevation and visible to the street shall feature trim surrounds (minimum reveal of 1"), headers, or sills.

o *Mechanical Equipment*

Mechanical equipment, such as air conditioners, heaters, evaporative coolers and other such devices are not permitted to be mounted on any roof.

All mechanical equipment shall be located in the side yard or in the rear yard when the side yard is less than 7' wide.

Mechanical devices, such as exhaust fans, vents, and pipes shall be located on the rear side of roof ridges, when possible, and painted to match adjacent roof structures.

All mechanical equipment shall be screened from public view in a manner that is compatible with the architectural style of the residence such as with landscaping, fencing, or walls.

Downspouts or rain water leaders shall be located on the inside corners of the structure.

o *Materials*

The front facades of homes should incorporate detail elements that are consistent with the architectural style of the residence. Detail elements may include shutters, enhanced windowsills, decorative wood and iron railings, or decorative grille work.

Building materials complement the building form and define the architectural style of a residence. Craftsmanship shall be accentuated through the use of quality building materials. Building



materials shall be appropriate in their use and application, and shall be consistent with the home's architecture. Surface treatments and materials shall appear as part of the design (and not an add-on).

Accent materials shall be either natural or have a natural appearance, such as brick, stone, or Hardie-Plank.

- Colors

Color schemes complement the character and architectural style of the building while providing a distinctive streetscape. Color palettes shall avoid monotony and provide a variety of schemes that will promote visual diversity.

Within residential neighborhoods, color schemes shall appropriately reflect the style of a home and variation on the colors of homes on a block shall be encouraged. Gloss paints are strongly discouraged on the body of the house.



Color schemes that compliment each other

Compatible (not duplicate) color schemes shall be provided for homes on adjacent lots.

- Massing

Variation in building form provides diversity and visual interest to the neighborhood street scene.

Staggered building wall planes (particularly on front elevations and rear and side elevations that face heavily traveled corridors) add a visually pleasant dimension to the building.

Projections and recesses provide shadow and depth.



Staggered building wall planes, or "massing"

Combinations of one- and two-story elements vary mass and enhance building articulation.

Single-story elements on front elevations enhance pedestrian scale of residential areas.

Two-story dwelling units shall include a substantial single story element adjacent to major collector or arterial streets or on corner lots to give a lower, more human scale at the edge of the street and corners.

- Roof

Roof treatments shall be consistent with the architectural style of the building. Roofs in residential neighborhoods shall be varied and may include gables, cross-gables, sheds, hips, or a combination of these roof forms.

All buildings shall have a variation in roof lines, ridge heights, roof forms, and direction of gables.

Gutters and downspouts may function as architectural elements, and shall be designed with the architecture of the structure.

Exposed gutters and downspouts shall be colored to match or compliment the surface to which they are attached.

- Porches and Patios

Porches shall be located on the front elevation to enhance the entry where applicable.

All porches should be fully covered with a roof, trellis structures, or second floor balconies.

Full front porches and wrap-around porches are encouraged.

Rear patio covers and second story balconies provide an opportunity for the articulation of rear facades, particularly along public spaces or streets.

4. Development Guidelines - Multiple Family Dwellings (MDR/HDR)

- Site Design

New multiple family residential developments shall respect the scale and character of the adjacent residential neighborhood through attention to building scale and orientation, proximity to adjacent uses, location of driveways, noise, lighting, and landscape.

Exterior site design and landscaping shall provide functional recreational space and/or community site amenities. Exterior spaces shall be designed to enhance the overall appearance and compatibility of such development by providing privacy, buffering, and daylight, and to provide a pleasant transition to the street.

Materials selected for multi-family projects shall be very durable and require low maintenance.

All trash enclosures shall be constructed of sturdy, opaque materials (with trash receptacles screened from view) which are in harmony with the architecture and materials of the main buildings.

Setbacks shall be encouraged to maximize varied setback from the public right-of-way.

When possible, buildings shall be organized around open spaces and gathering areas. Courtyards and interior patios are encouraged.

Buildings that are not oriented towards streets or around open/community spaces shall be well integrated into the neighborhood's overall design.

Buildings shall be oriented to provide some privacy yet still relate to the street and adjacent communities. Doors shall be visible from the street or common open space. Windows shall provide "eyes on the street."

Each dwelling unit shall have 100 square feet of open space that can be used for active or passive outdoor activity. Shared open space may be centralized or adjacent to units. Open space may include swimming pools, patios, decks, lawns, or courts. Builders are encouraged to centralize the open space or connect a series of open spaces that vary in shape, appearance, and usage.

Private open space should be allocated per dwelling unit. Private space may include patios, balconies, decks, or other similar uses. The private open space allocation may be counted towards the common open space requirement. However, boundaries between private and common open spaces shall be clearly defined by low walls or plant materials.

- *Building Design*

- *Façade*

- All support buildings and building facades should be articulated using color, arrangement, or change in materials to emphasize the façade elements.

- The planes of the exterior walls may be varied in height, depth, or direction.

- Extremely long facades shall be designed with sufficient building articulation, reveals, and, in some cases, landscaping to avoid a monotonous or overpowering institutional appearance.

- Accessory structures shall complement the architecture, color, and materials of the primary buildings.

- Building Scale

Buildings shall incorporate smaller-scale architectural forms such as bays, balconies, cantilevers, and dormers to visually reduce the height and scale of the building.



Overhangs and archways minimize the perceived scale of townhomes

Building heights shall be varied to give the appearance of a collection of smaller buildings.

Builders are encouraged to set second floors further back than first floors.



Example of varied rooflines, color schemes, and architectural details

Building Materials

Multi-family homes shall have a consistent theme that is emphasized through the building materials. Materials shall be durable and require low maintenance.

The neighborhoods shall blend to adjacent land uses.

- Colors

Color schemes should complement the character and architectural style of the building while providing a distinctive streetscape.

Color palettes shall avoid monotony and provide a variety of schemes that will promote visual diversity.

- Roofs

Roof pitches and materials shall be similar to other roof types in the neighborhood.

Roof lines shall be varied across the horizontal plane to create variation and visual interest.

Carports and garages shall have roofs that are similar to main buildings.

- Entries

Doors and gates to courtyards are important architectural features of multi-family neighborhoods.

Unit entries shall be individualized and emphasized.



Archways highlight this HDR building's entries

Units shall have individual porches, steps, or landscaped walkways.

- Trash Enclosures

Trash enclosures and recycling storage areas should be located in convenient but not prominent areas, such as inside parking courts or at the end of parking bays. Enclosures shall be well screened

Screening should be of the same material type as, or complimentary to, the material used on the main building.

Enclosures will be landscaped whenever feasible.

Trash receptacles will be accessible for trash collection but will neither block circulation drives nor conflict with parking.

Trash enclosures will not create blind spots or hiding areas.

- Circulation, Parking & Loading

Parking areas should be located in the development's interior area and not along street frontages. One large parking area where cars would dominate views and increase perceived density should be avoided.

Parking areas should be divided into a series of small parking courts with convenient access that relates to adjacent dwelling units.

For security reasons, dwelling units should have direct views to the parking areas; these views should be partially filtered through the use of appropriate landscaping (such as trees).

Parking structures, such as garages and carports, should be located where they do not obstruct natural surveillance.

Carports and tuck-under parking should not be visible from a public street.

Carports, detached garages, and accessory structures should be designed as an integral part of the development's architecture. Elements should be similar in material, color and detail to the main building of the development.

Prefabricated metal carports are prohibited.

Visitor and disabled parking should be clearly identified and distributed throughout the development to provide convenient access to groups of dwellings and community facilities.

- Pedestrian Circulation

- Convenient pedestrian connections should be provided to adjoining residential developments, commercial projects, and other compatible land uses.

- Pedestrian access to adjacent existing or planned open space areas and corridors should be provided for the development's residents.

E. Public Facilities

1. Street Rights-of-Way Guidelines

- Objectives

- Adopt a Community design that reduces off-site travel and trip lengths by efficiently linking residential, employment and commercial areas via the street network, bike trails and pedestrian walks.
- Promote safety by designing street layouts that minimize high-speed traffic through neighborhoods.
- Utilize road sections that are compatible with existing City road design standards, although project-specific conditions may necessitate some variation (with approval by the City Engineer).

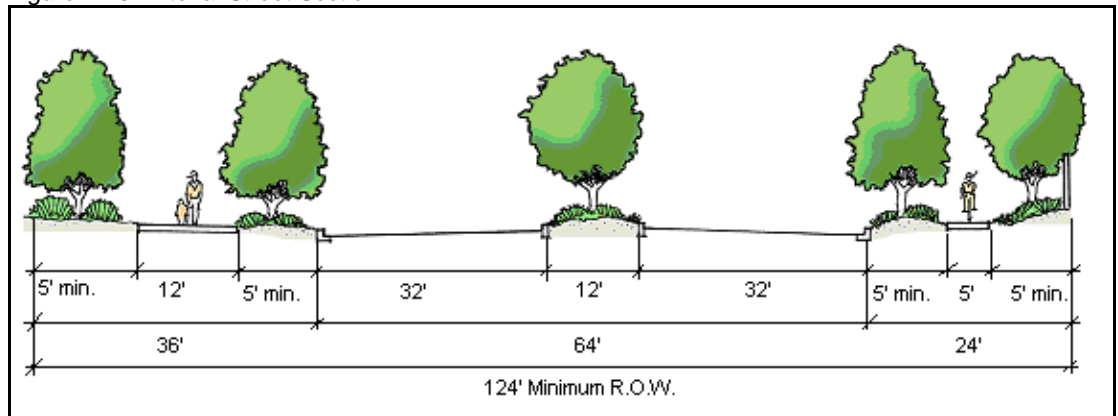
- Street Sections

- Street Rights of Way will be maintained by the City of Manteca. The below Street Sections are conceptual, and at the time of development will need to be reviewed and approved by the Engineering Division prior to construction.

- Arterial Street

Arterial streets will have 124' right-of-way (ROW) and include two travel lanes in each direction, greenways on each side including a 12' wide multi-use trail on one side of the street and a 5' wide sidewalk on the other side of the street, and soundwalls as necessary.

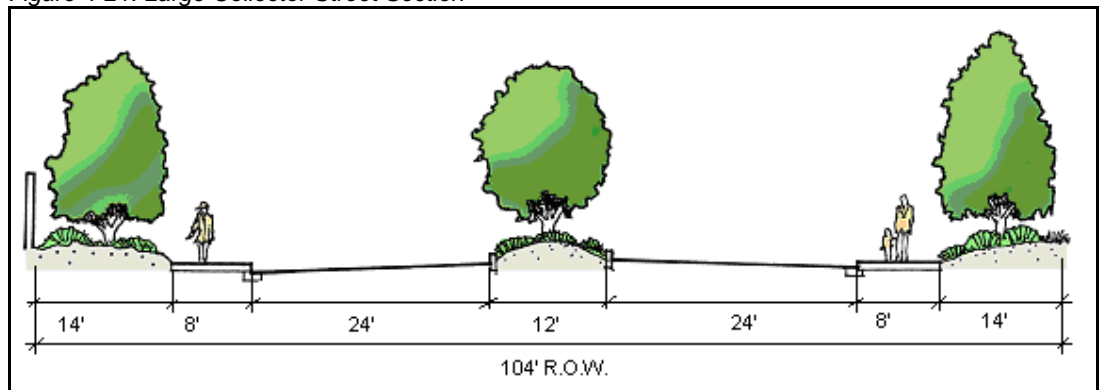
Figure 4-20: Arterial Street Section



- Large Collector Street

Large collector roads will have 104' right-of-way (ROW) including two travel lanes in each direction, 8' sidewalks, curb and gutter, 14' landscaping on each side, and soundwalls as necessary.

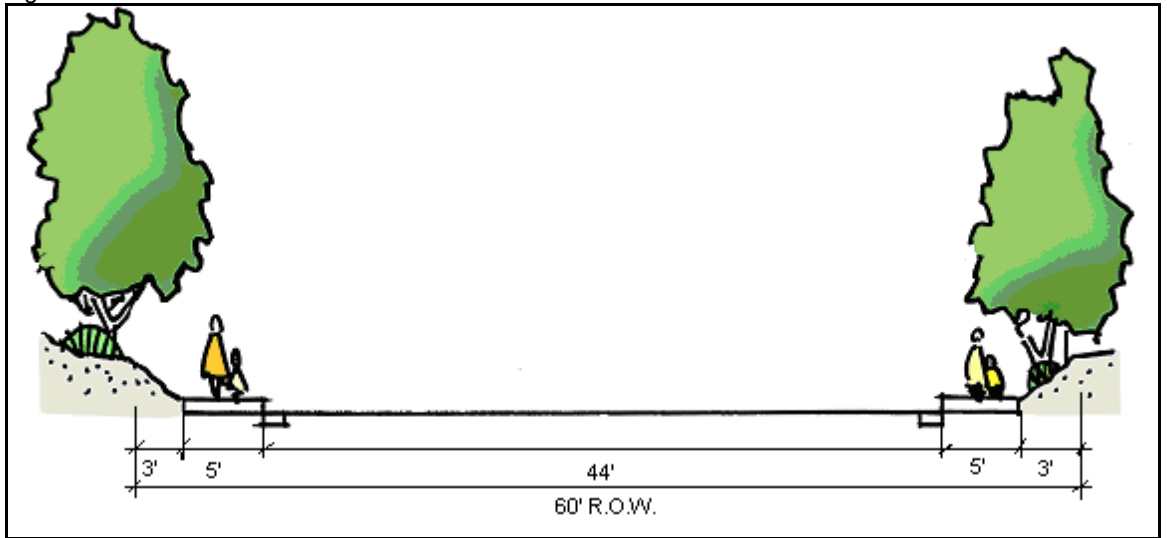
Figure 4-21: Large Collector Street Section



- Small Collector Street

Small Collector Streets will have 60' right-of-way, including one travel lane in each direction, 5' sidewalks, curb and gutter, planting strip, and soundwalls as necessary.

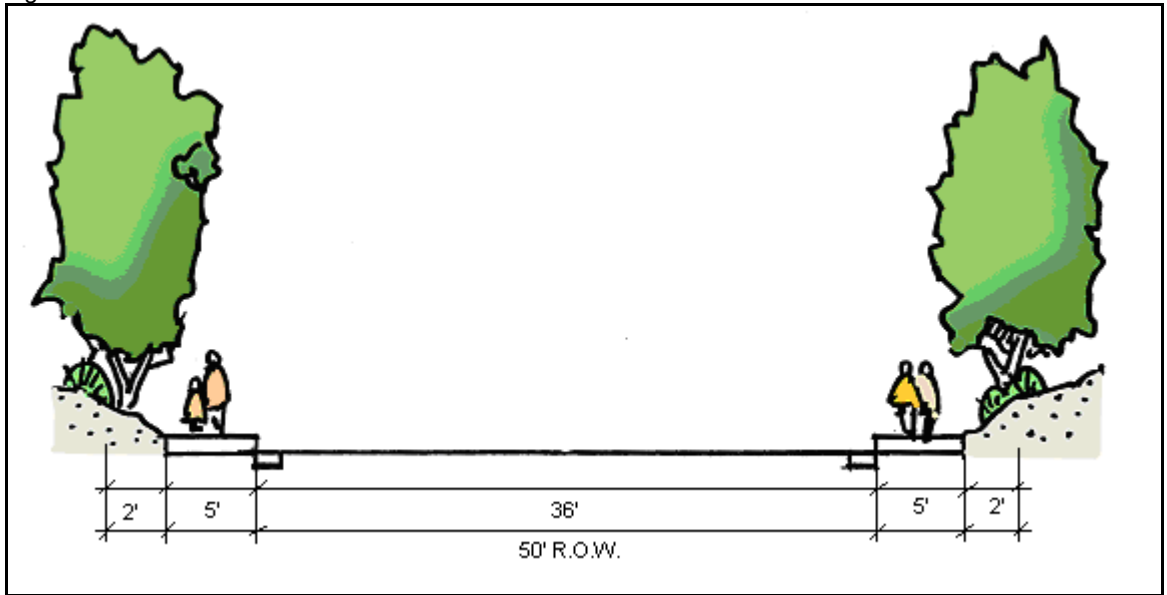
Figure 4-22: Small Collector Street Section



- Residential Street

Residential streets will have 50' right-of-way, including one travel lane in each direction, 5' sidewalk, curb and gutter, and 2' planting strip.

Figure 4-23: Residential Street Section



- Private Alley

Alleys will be 30' from face of building to face of building one travel lane and have tree planting cutouts at the edges.

- Streetscapes

The design of the streetscape should integrate, in a consistent and creative manner, plant materials, paths, landforms, soundwalls, lighting, furniture, and signage to produce an attractive and functional environment.

All landscaping should employ a mix of trees, shrubs, groundcovers, and turf where appropriate. The plant palette should be relatively limited and applied in groupings of similar species rather than a few plants of many different species planted together. At least one (1) tree shall be provided for each 2,500 square feet of landscaped area.

Street tree plantings shall be symmetrical and of the same species in the parkways on both sides of the streets.

One tree species or pattern of species shall be planted consistently at regular intervals along the entire length of a street for visual continuity. Spacing interval shall be no greater than twenty-five (25) feet on center (where practical).

Where trees are planted in medians, the plantings shall be continuous and at regular intervals. Spacing intervals shall be no greater than twenty-five (25) feet on center (where practical).

Landscaping shall include canopy trees. The location and spacing of trees is dependent on the species and light fixture locations. Please refer to Appendix A for a list of recommend trees for the Project Area.



Canopy-type street trees planted at regular intervals

2. Parks and Community Facilities

▪ Objectives

The following Parks and Recreation Plan objectives have been established to further guide the implementation of the Plan Area:

- Provide a range of parks and open space uses for passive and active recreation uses.
- Strategically locate parks near the centers of neighborhoods to facilitate access, to greet residents and visitors to individual neighborhoods, and to foster neighborhood gathering.
- Distribute recreational opportunities equitably throughout the community.

▪ Park Development Guidelines

The Community will provide at least five acres of park space per 1,000 residents, in conformance with the policies of the City of Manteca's General Plan.

Community and Neighborhood parks shall include dual-use storm water detention areas to accommodate portions of the project's storm-drainage needs.

Dual-use park and storm water detention facilities are to be fully landscaped and designed with gentle slope and varied contours to blend in with the adjacent park design.

The Community Park should be located adjacent to the Exposition Area so as to utilize shared parking areas.

The Community Park should be designed to accommodate both sports fields and the requirements of the storm drain detention facility.

Neighborhood parks should be located within residential neighborhoods. Basic facilities found in each park will include tot lots and children's play areas, sitting areas, and lawn areas large enough for non-competitive lawn sports. Storm drainage detention facilities may be located within parks, where necessary.

Subject to final review and approval by the City Parks and Recreation Department, neighborhood parks shall have low volume interior residential public streets on all sides.

Play areas closer than 25' to a street or parking lot should be surrounded by a fence.

The design for each park and open space area will be developed in keeping with City of Manteca's design criteria and standards and will be designed in partnership with the City of Manteca.

All improvements will be constructed from plans approved by the City and will be maintained by the City's Public Works Department.

- Lighting and Furniture

- Lighting

Lighting should be placed where it can best aid in illuminating activity areas. The site should not be overly lit. Electroliers should be scaled in size to match the size of areas to be lit.

Area lighting should be directed predominantly downward and shall be placed to prevent glare or excessive spray of light on neighboring sites.

Accent illumination should be provided at key locations such as building entries and driveway entries.

Pedestrian walkways, plazas, or other activity points should be illuminated.

Lighting or highlighting of building facades is permitted but should be sensitive, subtle, and not excessive.



Parking and roadways should use either mercury vapor or high pressure sodium lamps.

- Furniture

Benches, bollards, trash receptacles, and other furnishings should be in appropriate locations, such as along pedestrian walkways and at bus stops and similar sites in the open space network.

All furnishings should be attractive and resistant to weather and vandalism.

Recycled plastic material should be used in place of wood wherever possible.

Parks should use in-ground mounted site furniture.

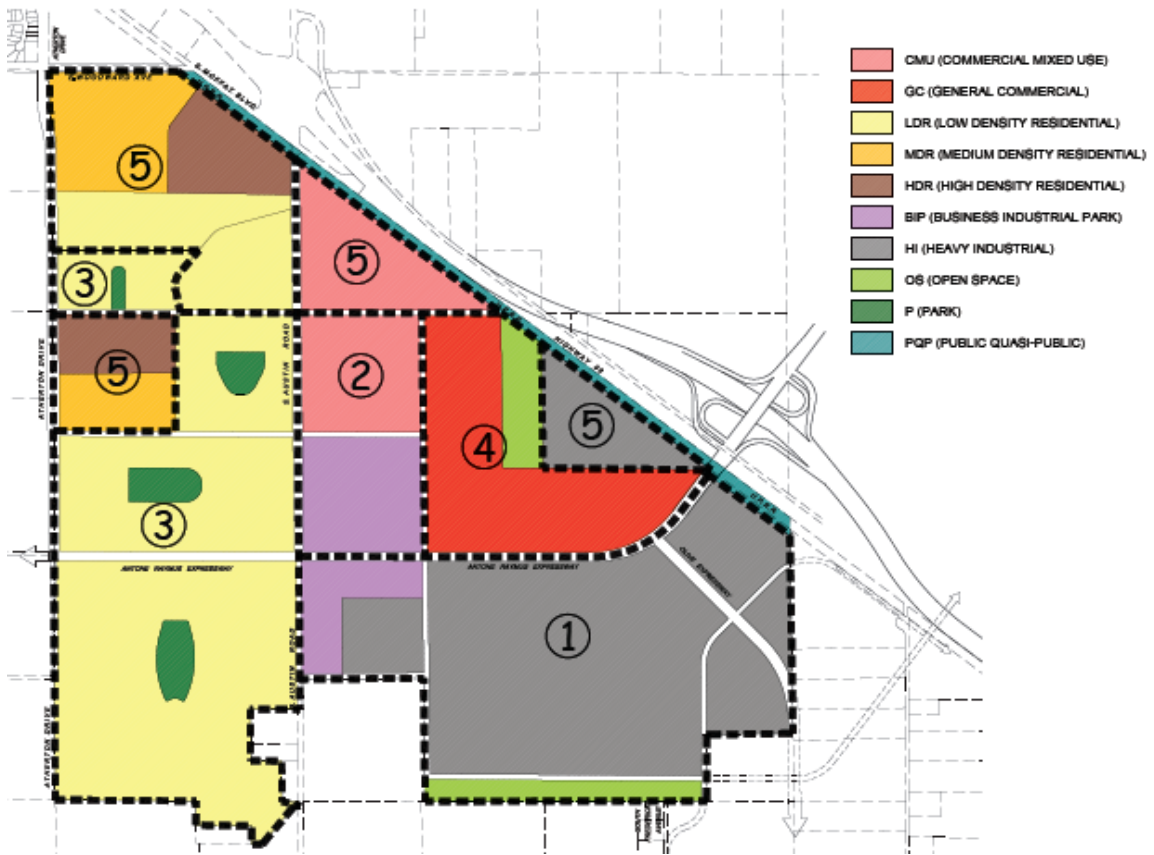
5. Phasing Plan

A. Project Phasing

It is anticipated that the project would be developed in several phases. The first phase is would include approximately 238 acres of heavy industrial (HI), 18 acres of Open Space and 24 acres of business/industrial/professional (BIP). Phase one is located in the southeastern portion of the site, south of the McKinley Avenue extension. Phase two totals approximately 82 acres, including the remaining 41 acres of BIP, and approximately half of the commercial mixed use (CMU). Phase three would total approximately 315 acres consisting of low-density residential (LDR) as well as park and open space. The fourth phase would encompass the general commercial (GC) portion of the site, totaling approximately 108 acres. The fifth phase includes various densities of residential as well as commercial and industrial uses. Since the properties in the fifth phase are either under Williamson Act contract or have existing uses, there are no immediate plans for development of these parcels, and therefore would be the final phase of the project. A phasing map is shown on Figure 5-1.

Figure 5-1: Phasing Map

AUSTIN ROAD BUSINESS PARK & RESIDENTIAL COMMUNITY PHASING PLAN



6. Consistency with City Plans/Policies

The Austin Road Master Plan will provide a framework for the development of heavy industrial, business/industrial and commercial uses and related facilities. The Master Plan must be consistent with City policies in order to ensure an orderly and appropriate pattern of growth. The following discussion focuses on consistency of the Master Plan with the Manteca General Plan, the Manteca Municipal Code, and the City's Sphere of Influence.

A. General Plan Consistency

The Master Plan responds affirmatively to the following policies and implementation programs of the Manteca General Plan 2023:

Land Use Policy LU-P-2

Growth must contribute to a strong diversified economic base and an effective balance between employment and housing opportunities for all income levels.

Land Use Policy LU-P-19

The City shall promote and assist the maintenance and expansion of Manteca's commercial sector to meet the needs of both Manteca residents and those living within Manteca's market area.

Land Use Policy LU-P-30

The City shall promote, cooperate in, and assist in the maintenance and expansion of Manteca's industrial sector employment development within the City of Manteca and in the south San Joaquin County area that will help reduce the home-to-work commute distance for Manteca residents.

Land Use Policy LU-P-46

The City shall allow for higher density housing opportunities along major streets so as to provide residents with access to the public transit system.

B. Consistency with Zoning Ordinance

The Master Plan itself will be adopted as part of the Manteca Municipal Code for the area of the City included in the Master Plan planning area. The Master plan contains information on mandatory requirements and design guidelines that, in some instances, depart from development requirements in the Manteca Municipal Code. Where differences occur, the Master Plan mandatory requirements and design guidelines shall prevail.

C. Consistency with Manteca Sphere of Influence

The entire Master Plan planning area is within Manteca's 10-year Planning Horizon Sphere of Influence approved by the San Joaquin County of Local Agency Commission on August 15, 2008.

APPENDIX 1. RECOMMENDED TREE LIST

Recommended Street Trees for Public Facilities	
Common Name	Botanical Name
Street Trees:	
Chinese Pistache	<i>Pistachia chinensis</i>
Coast Live Oak	<i>Quercus agrifolia</i>
Gingko	<i>Gingko biloba</i>
Golden Rain Tree	<i>Koelreuteria paniculata</i>
Hackberry	<i>Celtis australis</i>
Little Leaf Linden	<i>Tilia cordata</i>
Red Horsechestnut	<i>Aesculus x carnea</i>
Red Oak	<i>Quercus rubra</i>
Red Sunset Maple	<i>Acer rubrum</i> 'Red Sunset'
Sawleaf Zelcova	<i>Zelcova serrata</i>
Southern Live Oak	<i>Quercus virginiana</i>
Swamp Myrtle	<i>Tristania laurina</i> 'Elegant'
Accent Trees:	
Crape Myrtle	<i>Lagerstroemia</i> – Fauriei hybrids only
Flowering Cherry	<i>Prunus serralata</i>
Red Leaf Plum	<i>Prunus blireiana</i>
Recommended Trees for Parks	
Common Name	Botanical Name
Large Trees:	
Chinese Pistache	<i>Pistachia chinensis</i>
Coast Live Oak	<i>Quercus agrifolia</i>
Gingko	<i>Gingko biloba</i>
Golden Rain Tree	<i>Koelreuteria paniculata</i>
Hackberry	<i>Celtis australis</i>
Italian Stone Pine	<i>Pinus pinea</i>
Little Leaf Linden	<i>Tilia cordata</i>
Red Horsechestnut	<i>Aesculus x carnea</i>
Red Oak	<i>Quercus rubra</i>
Red Sunset Maple	<i>Acer rubrum</i> 'Red Sunset'
Sawleaf Zelcova	<i>Zelcova serrata</i>
Southern Live Oak	<i>Quercus virginiana</i>

Swamp Myrtle	Tristania laurina 'Elegant'
Accent Trees:	
Crape Myrtle	Lagerstroemia – Fauriei hybrids only
Flowering Cherry	Prunus serralata
Red Leaf Plum	Prunus blireiana
Recommended Trees for Residential Areas	
Common Name	Botanical Name
Street Trees:	
Autumn Blaze Maple	Acer x freemanii 'Autumn Blaze'
Chinese Pistache	Pistachia chinensis
Evergreen Elm	Ulmus parviflora sempervirens
Hackberry	Celtis australis
Holly Oak	Quercus ilex
Red Oak	Quercus rubra
Red Sunset Maple	Acer rubrum 'Red Sunset'
Sawleaf Zelcova	Zelcova serrata
Southern Live Oak	Quercus virginiana
Accent Trees:	
Crape Myrtle	Lagerstroemia – Fauriei hybrids only
Flowering Cherry	Prunus serralata
Red Leaf Plum	Prunus blireiana
Flowering Crabapple	Malus floribunda
<i>* Please note in addition to those trees listed above, request for alternative trees will be considered by the City Arborist.</i>	

APPENDIX 2. MEMORANDUM OF UNDERSTANDING BY AND BETWEEN THE CITY OF MANTECA AND THE CITY OF RIPON:

MEMORANDUM OF UNDERSTANDING
BY AND BETWEEN THE CITY OF MANTECA
AND THE CITY OF RIPON

This Memorandum of Understanding is entered into by and between the City of Ripon and the City of Manteca.

RECITALS

WHEREAS, the City of Ripon and the City of Manteca are communities in Southern San Joaquin County that are in close proximity to each other; and

WHEREAS, each City is required, by State Law, to have a General Plan; and

WHEREAS, the City of Ripon adopted a comprehensive General Plan on September 19, 2006; and

WHEREAS, the City of Manteca adopted a comprehensive General Plan on October 6, 2003; and

WHEREAS, said General Plans include a General Plan Boundary, Land Use Diagram and Policy Statement; and

WHEREAS, there are areas of overlap between the General Plan Land Use Diagrams, specifically: 1) south of State Highway 120 and north of State Route 99, and 2) west of State Route 99 easterly of South Austin Road, as set forth in Exhibit "A."

WHEREAS, the City of Ripon and City of Manteca desire to work cooperatively in their respective planning processes to efficiently and economically address planning and land use issues within their jurisdictions.

NOW, THEREFORE, it is mutually understood that:

1. The City of Ripon and the City of Manteca will work together to amend their respective General Plan Boundary and Land Use Diagrams in the vicinity of Austin Road and Graves Road to reflect the cooperative exchange of planning land area as set forth in

Exhibit "A" and to recognize that said areas will be requested to be included within the respective City's Sphere of Influence or Area of Interest as may be adopted by the Local Agency Formation Commission (LAFCo) of San Joaquin County.

2. The City of Ripon and the City of Manteca will work together to establish a Policy Statement in each City's General Plan to provide for cooperative planning efforts along the common General Plan Boundary of both cities.

3. The Policy Statement for each City will include provisions for the extension and connection of the pedestrian and bikeway systems of both cities along the State Highway 99/Union Pacific Railroad corridor.

4. The issues of joint concern and the parties' understandings as to how those issues are to be addressed will be adhered to by both parties inasmuch as such issues are discussed in this Memorandum of Understanding.

"CITY OF RIPON"

Attest:



City Clerk


Dated: 7-18-08



Mayor

Dated: 07-18-08

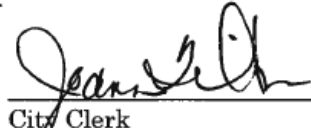
APPROVED AS TO FORM:



City Attorney

"CITY OF MANTECA"

Attest:



City Clerk


Dated: 07/23/08



Mayor

Dated: 7/23/08

APPROVED AS TO FORM:



City Attorney

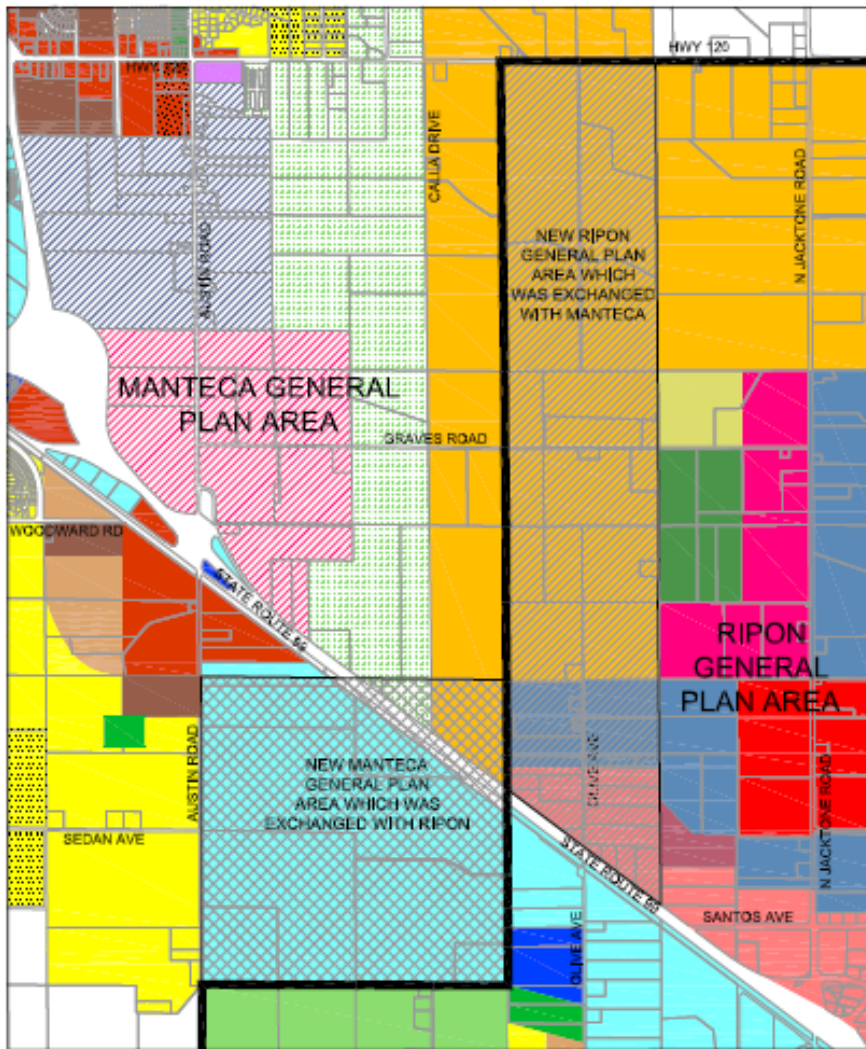


EXHIBIT A
 MANTECA / RIPON GENERAL PLAN
 EXCHANGE AREA

