

8. TRANSPORTATION

This chapter provides PFIP information as it pertains to the City transportation system. A summary of the information is as follows: A) Background; B) Facilities and Costs; C) Dwelling Unit Equivalents; D) Fee Methodology; E) Fee Schedule; and F) Construction Responsibilities.

A. BACKGROUND

The City of Manteca's transportation system provides for the mobility of people and goods, and influences patterns of growth and economic activity. The *Transportation PFIP Plan*, as the implemented document for the City of Manteca's General Plan Transportation Policies, was created to ensure that public facilities are adequate as the City of Manteca grows and develops in accordance with its General Plan. The *PFIP Plan* provides a tool to equitably finance transportation infrastructure (streets, intersections, and interchanges) which is associated with new development growth through the year 2023.

Key objectives of the Transportation PFIP Plan include the following:

- Identify citywide transportation improvements, to be implemented, that serves new growth areas, and is financed by new growth.
- Identify which citywide transportation improvements are to be incorporated into the *PFIP Plan* allowing estimates to be prepared that are quantified, costed, and financed accurately for each segment.
- Identify and convert land use categories into Dwelling Unit Equivalents (DUE's). The DUE calculations are based on a current traffic study and are converted using trip based calculations. DUE's are further refined utilizing DUE factors based on combined trip analysis.
- Identify and establish the funding mechanism for the *PFIP Plan*.
- Execute the PFIP Plan to collect fees and issue reimbursements in accordance with the *PFIP Plan*.

The City of Manteca has prepared a development forecast through the end of the Calendar Year 2023. This development forecast has been updated as part of this document to focus on the specific transportation elements that will be constructed and funded through the impact fee model program. This forecast is presented in Section B and Plate A-1 – A-6 in Appendix A. Six planning zones have been identified to define the capital improvements needed to serve future growth: Zones 1, 2, 3, 4, 5 and 6. For the purposed of this fee model, all of the Zones have been combined into one single zone to balance the anticipated fees throughout the City of Manteca. See Plate E-1.01 in Appendix E for a depiction of the transportation zones.

B. FACILITIES AND COSTS

The transportation facilities included in the *PFIP Plan* include the streets, intersections, and interchanges located within the General Plan area that will facilitate new growth. These

facilities, along with their estimated construction and anticipated right-of-way acquisition costs, are identified in the City of Manteca Supplemental Public Facilities Implement Plan (PFIP) transportation supplemental fee estimate packages, dated October, 2017. Interchange costs are based on preliminary estimates provided by the City of Manteca.

To meet overall transportation goals and to ensure public safety, improvements must be developed along both sides of street segments and along the extents of intersections. In areas where the improvements adjoin groups of small parcels that are not likely to develop, due to the limitations in project size, the right-of-way will be purchased. In the case where improvements for these specified areas were constructed, and the project area is developed after the street improvements were constructed, the Developer will be subject to PFIP fees for the street section that was previously developed, even though the improvements are in place.

The City of Manteca Public Facilities Implementation Plan (PFIP) Transportation Fee Estimates and reference individual Street Segment and Intersection Exhibits are available for review at the City of Manteca Public Works Department.

1. Streets

Streets included in the *PFIP Plan* vary between two, four, and six lanes. All streets, and respective cross-section designations, are included in the *PFIP Plan* and are itemized in Plate E-1.03 in Appendix E. In all cases, the *PFIP Plan* will cover the cost for the center 14 feet of each roadway. This includes a 14 foot wide landscaped median with median curbs on each side. Additional interior lanes are included in the *PFIP Plan* for streets with more than two lanes. The reimbursable improvements covered in the *PFIP Plan* for streets are defined as follows:

- 2 Lane Street - A 14 foot wide landscaped median on a two lane street. For more information, see Plate E-2.01 in Appendix E.
- 4 Lane Street - A 14 foot wide landscaped median and two interior lanes. For more information, see Plate E-2.03 in Appendix E.
- 6 Lane Street - A 14 foot wide landscaped median and four interior lanes. For more information, see Plate E-2.06 in Appendix E.
- Airport Way North of Yosemite Avenue – A 36 foot wide landscaped median and 2 interior lanes. For more information, see Plate E-2.07 in Appendix E.
- Non-typical street sections are included in the individual street segment improvement exhibits to describe the specific improvements proposed at these locations.
- At specific locations noted in the individual street segment improvement exhibits, a portion of the required right-of-way and/or improvements are already in place as noted on these plans. No additional right-of-way and/or improvements are required in these areas. The proposed improvements should conform to existing improvements, protect existing curb, gutter, sidewalk, and landscaping throughout the course of construction, and repair/replace any damaged improvements.

The developer will only be reimbursed, by monetary or fee credit, for the improvements covered in the *PFIP Plan*.

The *PFIP Plan* does not include the outer landscape strip located between the curb and right-of-way line, the sidewalk located within the landscape strip, the curb and gutter, or the first travel lane adjacent to the curb and gutter. There will be no reimbursement for these improvements.

2. Intersections

Intersections included in the *PFIP Plan* vary between two, four and six lanes, and are designated as either Roundabouts, New Signals, Modified Signals, and Re-Striping Only / Loops. All intersections, and the respective designations, are included in the *PFIP Plan* and are itemized in Plate E-1.04 in Appendix E. In all cases the *PFIP Plan* will cover the cost for the center 14 feet of the roadway within the intersection transition area. This 14 foot wide area includes the proposed 12 foot turning lane, with a two foot wide curbed median, for standard intersections or the entire median for roundabouts. Additional lanes are covered for intersection legs with more than two lanes.

The *PFIP Plan* does not include the outer landscape strip located between the curb and right-of-way line, the sidewalk located within the landscape strip, the curb and gutter, and the first travel lane adjacent to the curb and gutter, except in the case where a dedicated right turn lane is encountered (which is included in the *PFIP Plan*), in which case the first thru travel lane toward centerline is the Developers' responsibility.

The improvements for intersections, which are included in the *PFIP Plan*, are defined as follows:

- 2 Lane Intersection, Single Turn Lane – The center 14 feet and the 12 foot dedicated right turn lane. For more information, see Plate E-2.02 and E-3.01 in Appendix E.
- 4 Lane Intersection, Single Turn Lane – The center 14 feet plus two interior thru travel lanes, for a total of 36 feet, and the 12 foot dedicated right turn lane. For more information, see Plate E-2.04 and E3.03 in Appendix E.
- 4 Lane Intersection, Double Turn Lane – The center 14 feet plus two interior thru travel lanes and an additional 12 foot left turn lane, for a total of 48 feet, and the 12 foot dedicated right turn lane. For more information, see Plate E-2.05 and E3.04 in Appendix E.
- 6 Lane Intersection, Single Turn Lane – The center 14 feet plus four interior thru travel lanes, for a total of 58 feet, and the 12 foot dedicated right turn lane. For more information, see Plate E-2.08, E3.05 and E3.06 in Appendix E.
- 6 Lane Intersection, Double Turn Lane – The center 14 feet plus four interior thru travel lanes and an additional 12 foot left turn lane, for a total of 70 feet, and the 12 foot dedicated right turn lane. For more information, see Plate E-2.10 and E3.07 in Appendix E.
- 2 to 2 Lane Roundabouts - The center 14 feet on each approach to the intersection, including the roundabout footprint within the intersection. For more information, see Plate E3.08 in Appendix E.

- 2 to 4 Lane Roundabouts - The center 14 feet on each two lane road approach to the intersection; the center 14 feet plus two interior travel lanes on each four lane approach to the intersection; the roundabout footprint and the interior lane within the intersection. For more information, see Plate E3.09 in Appendix E.

3. Interchanges

Interchanges included in the *PFIP Plan* vary with regard to lanes, ramps, and geometric configurations. All included interchanges are identified in the *PFIP Plan* and are itemized in Plate E1.05 in Appendix E. The *PFIP Plan* will cover approximately 30% of the cost for each interchange and the remaining costs will be funded by other sources, mostly Federal and State funding, possibly with some local funding.

Fees are based on cost estimates prepared for each street segment, intersection, and interchange. Each development project will be responsible to construct the designated streets and intersections located along the project, from the newly dedicated right-of-way line, to the PFIP boundary located on the travel lanes opposite the project frontage. The project list is included in Plate E1.02 in Appendix E and the cost estimates are available for review at the City of Manteca Public Works Department.

C. DWELLING UNIT EQUIVALENTS

All PFIP transportation fees will be assessed based upon the Dwelling Unit Equivalent (DUE). A DUE factor was developed to convert the new trip generation rates, for each General Plan land use, based on a factor that converts commercial, industrial, etc. trips to an equivalent low-density, single-family dwelling unit value. It follows that the fee per unit of development is directly proportional to the DUE associated with each particular use. The Dwelling Unit Equivalent was developed as follows:

- A review of the base year SJCOG / City of Manteca Travel Demand Model was performed to confirm that land use totals within each benefit zone are accurate based on: Census data, GIS land use database, and aerial imagery. Several land use adjustments were made to reflect currently constructed land uses within certain zones.
- A review of the future year SJCOG / City of Manteca Travel Demand Model was performed to confirm land use totals within each benefit zone are consistent with the City of Manteca General Plan and proposed/approved Specific Plans.
- Calculations were performed to determine growth in dwelling units and businesses (classified as retail, service, or other), between base and future year version of the SJCOG / City of Manteca Travel Demand Model, for each of the six (6) benefit districts.
- Growth in land use was converted into DUEs for the following land use designations:
 - Single Family Residential
 - Multi-Family Residential (Apartments)
 - General Office / Business Professional
 - Medical Office Building
 - Community Commercial
 - Regional Commercial

- Church
- Clubhouse
- Hotel
- Business Industrial Park
- Industrial Park / R&D
- Distribution / High Cube Warehousing

The 2017 Transportation PFIP Technical Memorandum prepared by Fehr and Peers, and part of this report, provides additional details regarding how the various land uses are derived and how corresponding DUE factors were derived. Table 8-1 provides a summary of the DUE factor by land use type.

Table 8-1
Dwelling Unit Equivalent Factors

Land Use Category	Unit	Daily Trip Generation Rate	New Trips	DUE
Residential [a]				
Single Family Residential	Dwelling Unit	9.52	N/A	1.00
Multi-Family Residential (Apartments)	Dwelling Unit	6.65	N/A	0.70
Non - Residential [a]				
General Office / Business Professional	1,000 sq.ft.	11.03	68%	0.79
Medical Office Building	1,000 sq.ft.	36.13	68%	2.58
Community Commercial	1,000 sq.ft.	33.18	68%	2.37
Regional Commercial	1,000 sq.ft.	47.72	72%	3.61
Church	1,000 sq.ft.	2.38	50%	0.13
Clubhouse [b]	1,000 sq.ft.	22.88	50%	1.20
Hotel	Rooms	8.17	68%	0.58
Business Industrial Park [c]	-	-	-	-
Industrial Park / R&D	1,000 sq.ft.	6.83	77%	0.55
Distribution / High Cube Warehousing	1,000 sq.ft.	3.56	77%	0.29

[a] If a parking garage is to serve a specific Land Use Category then the PFIP fee would be included in the number of dwelling units or square footage for the project that the parking garage would serve.

[b] If the clubhouse is part of a master planned community, than the PFIP fee would be included in the number of dwelling units and would not be charged separately

[c] Business Industrial Park fee subject to Public Works Review

D. FEE METHODOLOGY

The City has established a fee structure that allocates costs to construct transportation infrastructure related to new development. There are two types of fees assessed, non-PFIP fees which have been adopted by separate City resolutions, and PFIP fees which are adopted in connection with this document. This section describes the non-PFIP and the PFIP fees, and briefly summarizes how project costs are developed. As the non-PFIP fees are adopted by a separate City resolution, policies related to these fees including fee adjustments are identified in other City documents. Descriptions of the non-PFIP fees are provided for informational purposes only.

1. Non-PFIP Fees

Federal, State, and local funding opportunities have not been identified at this time and will not be utilized for streets and intersections included in the *PFIP Plan*, but will be utilized to help fund the interchanges. Historically, Federal, State, and non-PFIP local funding sources have funded 70 percent of interchange costs. Based on that assumption, historical funding sources are anticipated to cover 70 percent of the interchange costs in this program.

2. PFIP Fees

Transportation fees are the only reliable source of financing, within the City's control, for funding the *PFIP Plan*. Transportation fees specific to the *PFIP Plan* will be collected to fund the *PFIP Plan* improvements. The PFIP fees are calculated utilizing cost estimates prepared for each street segment, intersection, and interchange included in the *PFIP Plan*. Detailed cost estimates were prepared for the street segments and intersections included in the *PFIP Plan*, in accordance with the cross sections and intersection exhibits specified in Appendix E. The estimated costs for the interchanges are based on engineer's estimates from the initial Project Study Report/Project Development Study (PSR/PDS). The *PFIP Plan* is responsible for funding 30 percent of the total interchange costs.

The PFIP fee is approximately equal to the total cost of all improvements divided by the total number of Dwelling Unit Equivalents (DUEs). The construction costs for the anticipated improvements have been combined into one single zone to balance the fees throughout the City of Manteca. Please reference table 8-2 for the appropriate PFIP Fee per land use category. Projects included in the *PFIP Plan* qualify for monetary or fee credit reimbursement.

E. FEE SCHEDULE

The PFIP fee methodology described in Section D is combined to establish the PFIP Transportation Fee. Table 8-2 summarizes the fees by Dwelling Unit Equivalent and associated land use in accordance with the combined transportation zones. Table E-1 and E-2 in Appendix E and the Supplemental Public Facilities Implement Plan (PFIP) Transportation Fee Estimate package identify the costs and calculations associated with the PFIP fees.

Table 8-2
Transportation PFIP Fee
Public Facilities Implementation Program Fees

Development fee for construction and maintenance of roadways and improvements.

Land Use	Fee	Unit
Residential [a]		
Single Family Residential	\$8,300	per dwelling unit
Multi-Family Residential (Apartments)	\$5,800	per dwelling unit
Non - Residential [b]		
General Office / Business Professional	\$2,180	per 1000 s.f. bldg
Medical Office Building	\$7,125	per 1000 s.f. bldg
Community Commercial	\$6,550	per 1000 s.f. bldg
Regional Commercial	\$9,950	per 1000 s.f. bldg
Church	\$1,038	per 1000 s.f. bldg
Clubhouse [b]	\$9,974	per 1000 s.f. bldg
Hotel	\$1,495	per room
Business Industrial Park [c]	-	-
Industrial Park / R&D	\$1,495	per 1000 s.f. bldg
Distribution / High Cube Warehousing	\$795	per 1000 s.f. bldg

Note: This information is based on a Citywide DUE fee
Total PFIP cost of \$215,807,783.12

[a] If a parking garage is to serve a specific Land Use Category then the PFIP fee would be included in the number of dwelling units or square footage for the project that the parking garage would serve.

[b] If the clubhouse is part of a master planned community, than the PFIP fee would be included in the number of dwelling units and would not be charged separately

[c] Business Industrial Park fee subject to Public Works Review

F. CONSTRUCTION RESPONSIBILITIES

The City and Developer have responsibilities for construction of transportation facilities as follows:

1. City Responsibilities

The City will be responsible for obtaining the 70 percent funding for the interchanges included in the *PFIP Plan*, and will also be responsible for plan administration, fee collection, reimbursement, and plan updates.

2. Developer Responsibilities

The Developer will be responsible for the design and preparation of construction documents as well as the construction of the transportation PFIP projects located along their project frontage. There may be cases where offsite PFIP improvements are essential, in which case the Developer may be required to construct the additional offsite improvements beyond the proposed project limits.

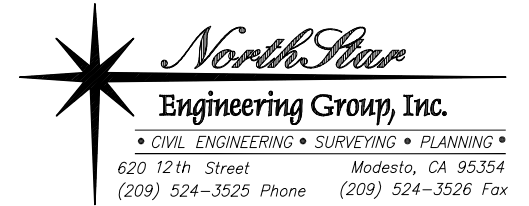
3. Construction Sequencing

Construction sequencing will be led by development efforts, which generally follow a logical sequence. However, it is anticipated that gaps in improvements may occur as projects develop that may lead to health and safety concerns. In such cases, a Developer may be required to construct additional portions of PFIP projects and apply for reimbursement, if funds are available, or fee credits. If reimbursement funds are not available, the Developer will be added to the list for future reimbursement.



























Appendix A*

Vacant Land Inventory

*These sheets should be referred to in conjunction with the Chapter 8 - Transportation Element Update only, background information for all other Chapters is based on the Appendix A documents in the adopted 2013 Public Facilities Implementation Plan Update



GENERAL PLAN DESIGNATION

-  AG (AGRICULTURE)
-  NC (NEIGHBORHOOD-COMMERCIAL)
-  CMU (COMMERCIAL MIXED USE)
-  GC (GENERAL COMMERCIAL)
-  VLDR (VERY LOW DENSITY RES.)
-  LDR (LOW DENSITY RES.)
-  MDR (MEDIUM DENSITY RES.)
-  HDR (HIGH DENSITY RES.)
-  BIP (BUSINESS INDUSTRIAL PARK)
-  LI (LIGHT INDUSTRIAL)
-  HI (HEAVY INDUSTRIAL)
-  OS (OPEN SPACE)
-  P (PARK)
-  PQP (PUBLIC/QUAZI-PUBLIC)
-  UR (URBAN-RESERVE)
-  UR-AG (UR-AGRICULTURE)
-  UR-CMU (UR-COMMERCIAL MIXED USE)
-  UR-GC (UR-GENERAL COMMERCIAL)
-  UR-VLDR (UR-VERY LOW DENSITY RES.)
-  UR-LDR (UR-LOW DENSITY RES.)
-  UR-MDR (UR-MEDIUM DENSITY RES.)
-  UR-BIP (UR-BUSINESS IND. PARK)
-  UR-LI (UR-LIGHT INDUSTRIAL)
-  UR-P (UR-PARK)
-  UR-PQP (UR-PUBLIC/QUAZI-PUBLIC)
-  DEVELOPED LAND

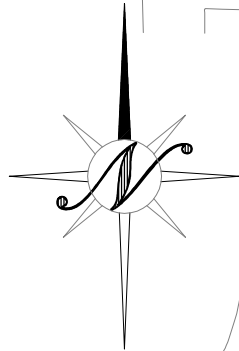
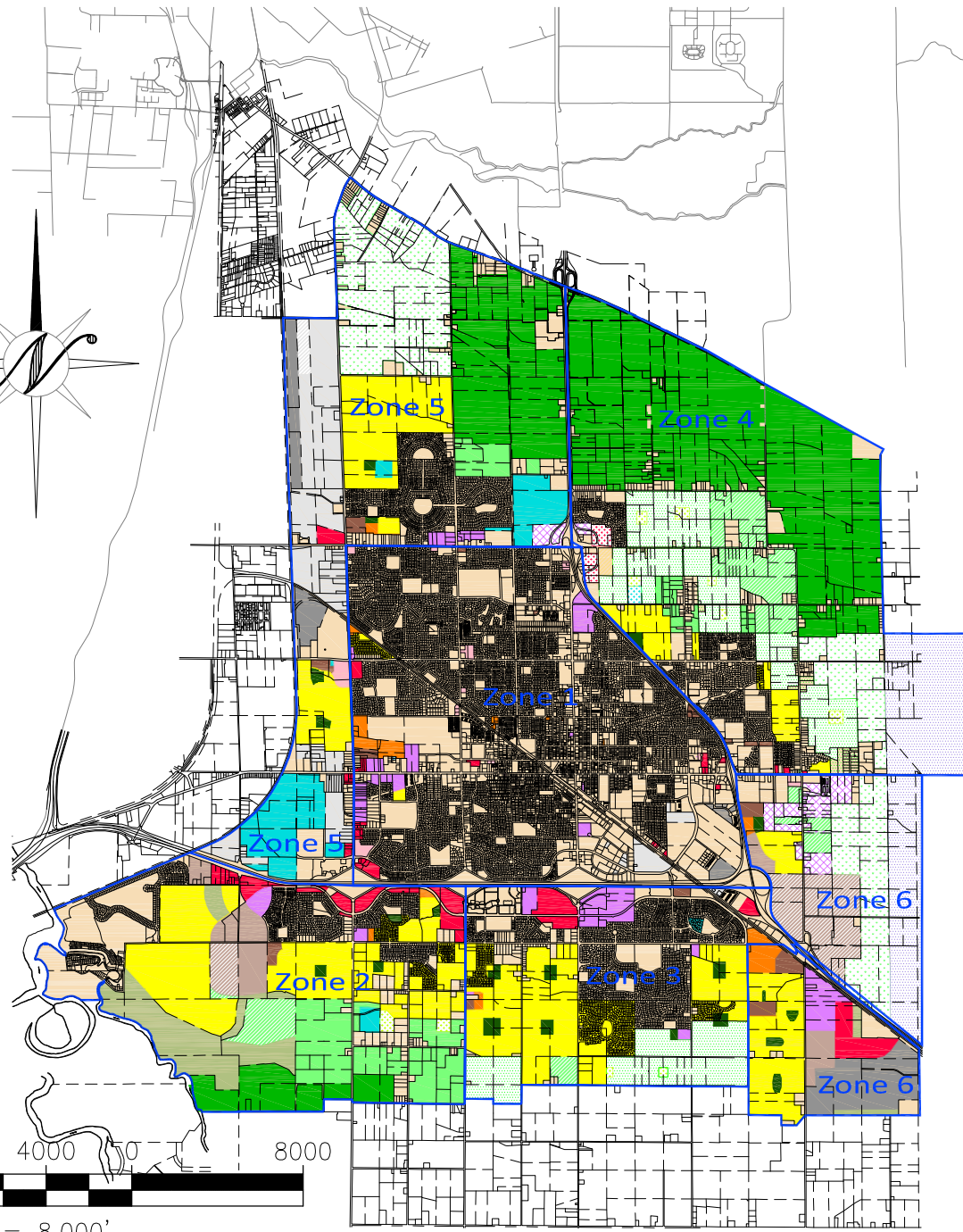
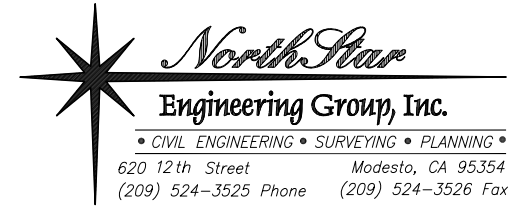












PLATE A-1

VACANT LAND





GENERAL PLAN DESIGNATION

	NC (NEIGHBORHOOD-COMMERCIAL) 31.32 AC
	BP (BUSINESS PROFESSIONAL) 0.57 AC
	CMU (COMMERCIAL MIXED USE) 123.52 AC
	GC (GENERAL COMMERCIAL) 50.97 AC
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	MDR (MEDIUM DENSITY RES.) 58.73 AC
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	HI (HEAVY INDUSTRIAL) 31.87 AC
	DEVELOPED LAND

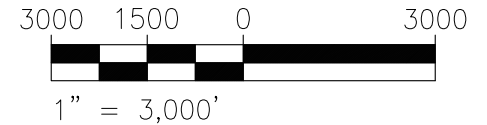
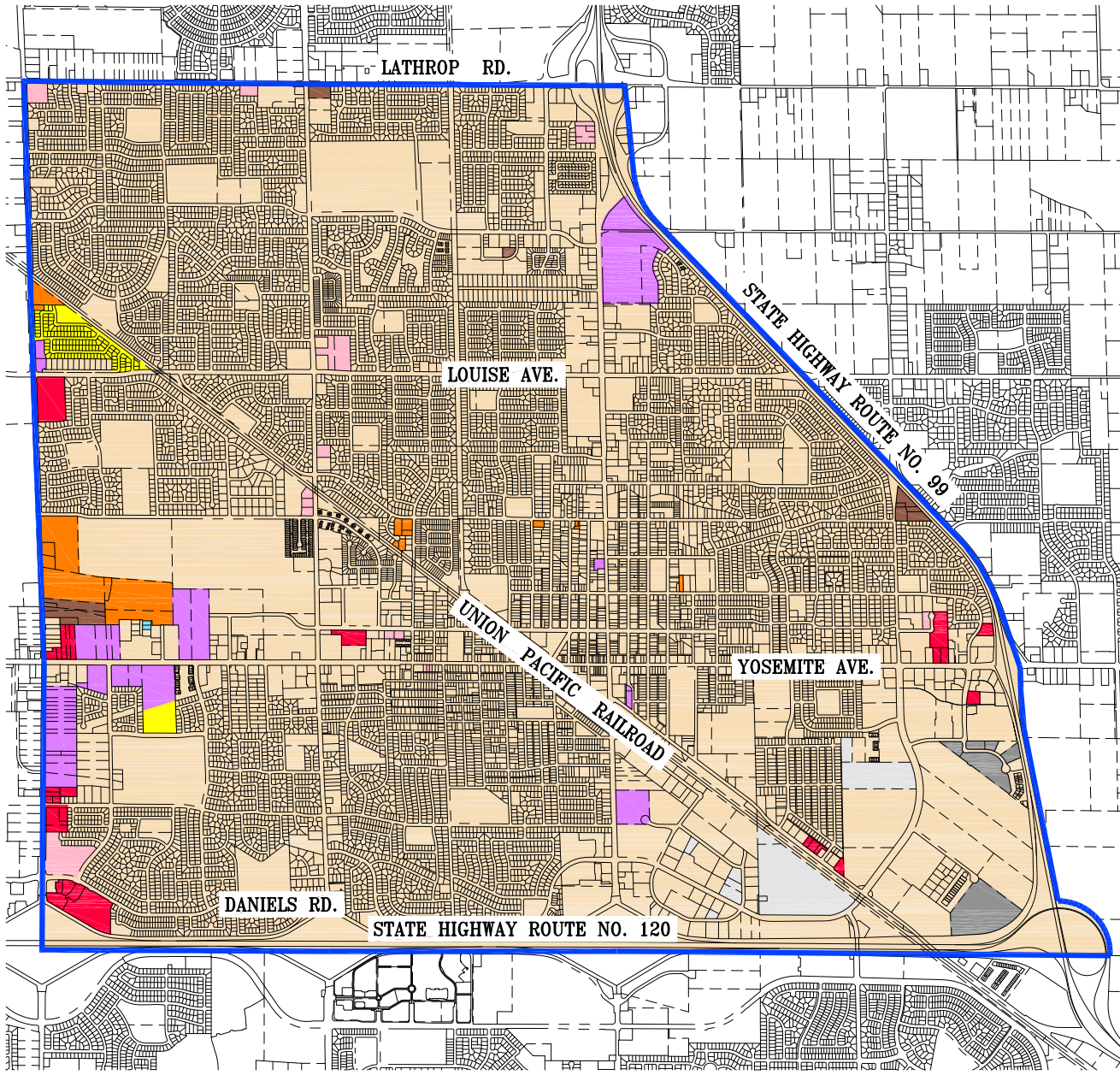


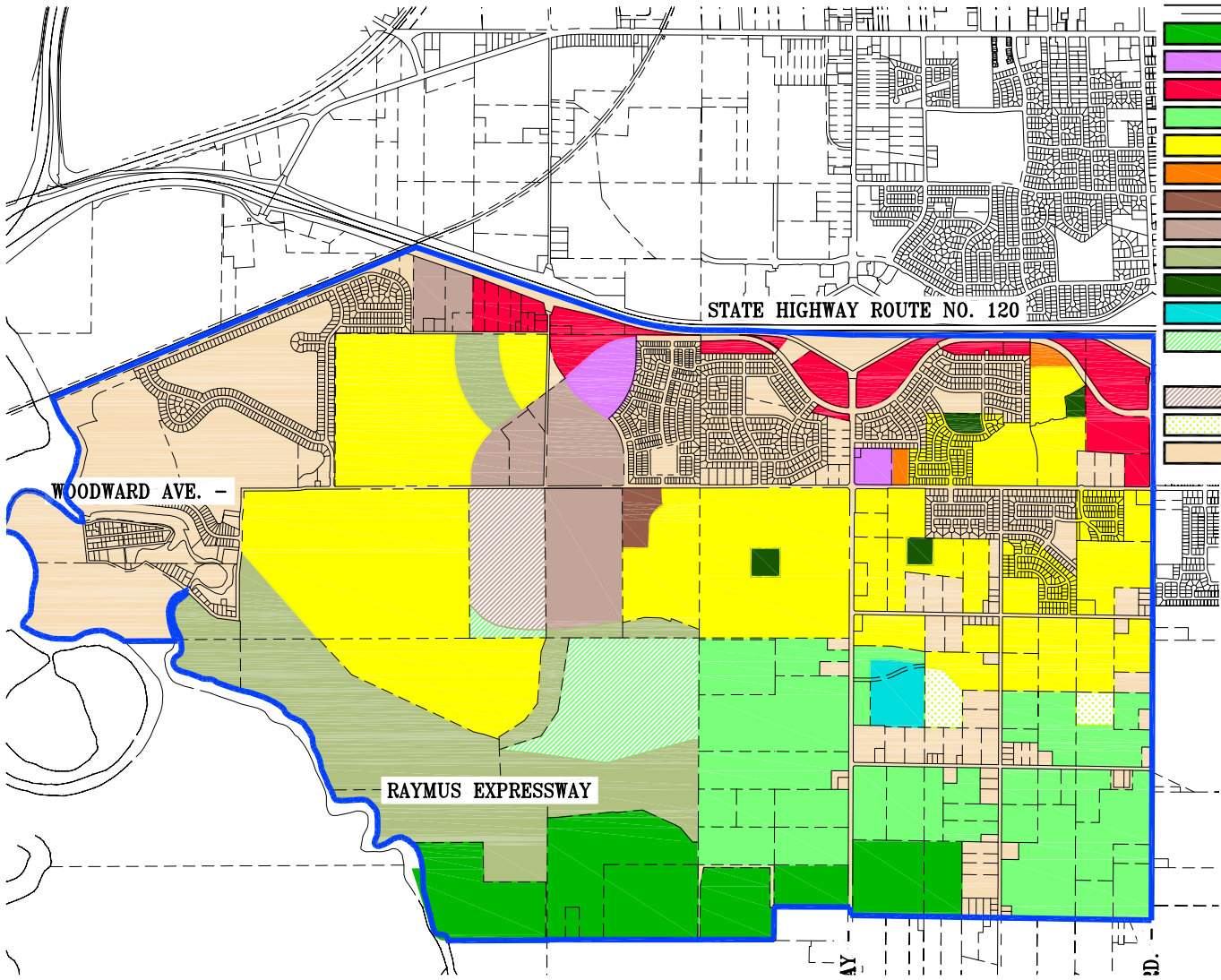
PLATE A-1.1
















ZONE 1



North Star
Engineering Group, Inc.
 • CIVIL ENGINEERING • SURVEYING • PLANNING •
 620 12th Street Modesto, CA 95354
 (209) 524-3525 Phone (209) 524-3526 Fax

GENERAL PLAN DESIGNATION



-  AG (AGRICULTURE) 305.34 AC
-  CMU (COMMERCIAL MIXED USE) 33.47 AC
-  GC (GENERAL COMMERCIAL) 144.25 AC
-  VLDR (VERY LOW DENSITY RES.) 518.43 AC
-  LDR (LOW DENSITY RES.) 1,044.87 AC
-  MDR (MEDIUM DENSITY RES.) 9.57 AC
-  HDR (HIGH DENSITY RES.) 13.17 AC
-  BIP (BUSINESS INDUSTRIAL PARK) 179.37 AC
-  OS (OPEN SPACE) 435.95 AC
-  P (PARK) 19.19 AC
-  PQP (PUBLIC/QUAZI-PUBLIC) 25.13 AC
-  UR-VLDR (UR-VERY LOW DENSITY RES.) 124.83 AC
-  UR-BIP (UR-BUSINESS IND. PARK) 72.24 AC
-  UR-P (UR-PARK) 22.85 AC
-  DEVELOPED LAND

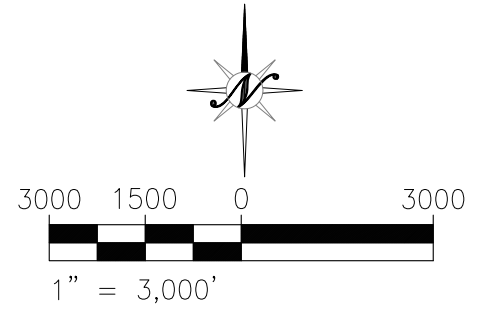
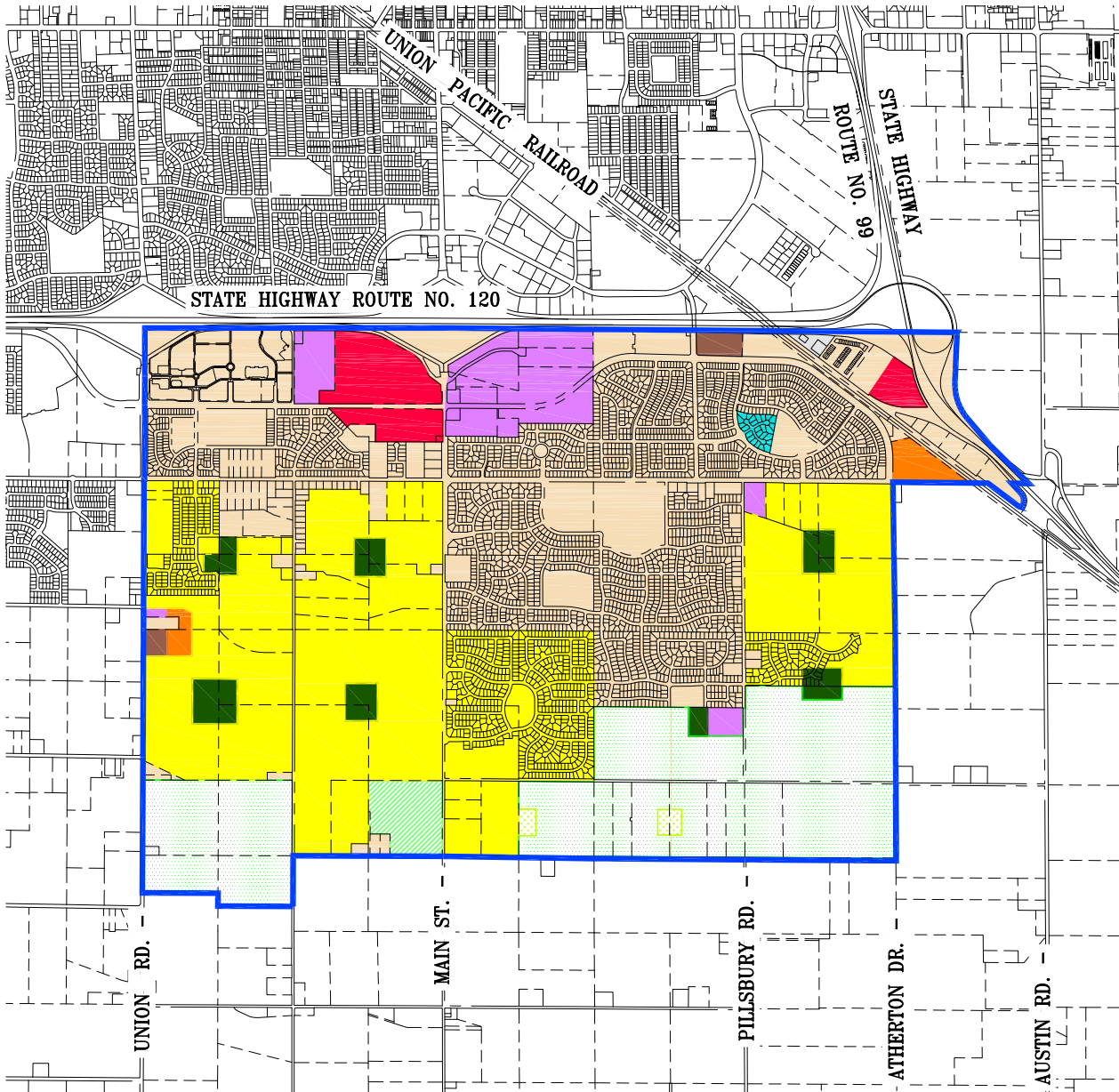
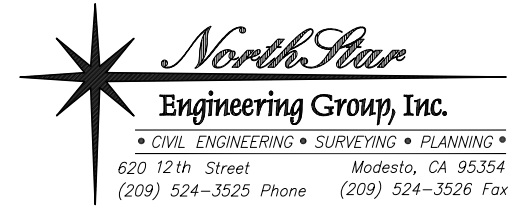


PLATE A-1.2

ZONE 2



GENERAL PLAN DESIGNATION

- CMU (COMMERCIAL MIXED USE) 129.30 AC
- GC (GENERAL COMMERCIAL) 76.20 AC
- LDR (LOW DENSITY RES.) 943.69 AC
- MDR (MEDIUM DENSITY RES.) 21.03 AC
- HDR (HIGH DENSITY RES.) 11.60 AC
- LI (LIGHT INDUSTRIAL) 3.92 AC
- P (PARK) 61.53 AC
- PQP (PUBLIC/QUAZI-PUBLIC) 7.53 AC
- UR-VLDR (UR-VERY LOW DENSITY RES.) 35.95 AC
- UR-LDR (UR-LOW DENSITY RES.) 483.52 AC
- UR-P (UR-PARK) 7.55 AC
- DEVELOPED LAND

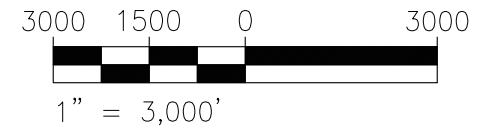


PLATE A-1.3

ZONE 3



GENERAL PLAN DESIGNATION

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	LDR (LOW DENSITY RES.) 222.10 AC
	HDR (HIGH DENSITY RES.) 7.83 AC
	P (PARK) 8.59 AC
	UR (URBAN-RESERVE) 604.36 AC
	UR-AG (UR-AGRICULTURE) 303.41 AC
	UR-CMU (UR-COMMERCIAL MIXED USE) 9.51 AC
	UR-GC (UR-GENERAL COMMERCIAL) 42.52 AC
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	UR-LDR (UR-LOW DENSITY RES.) 711.31 AC
	UR-MDR (UR-MEDIUM DENSITY RES.) 19.40 AC
	UR-P (UR-PARK) 38.15 AC
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	DEVELOPED LAND

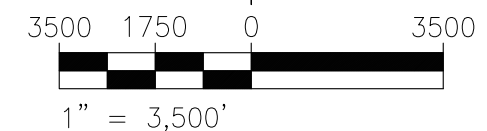
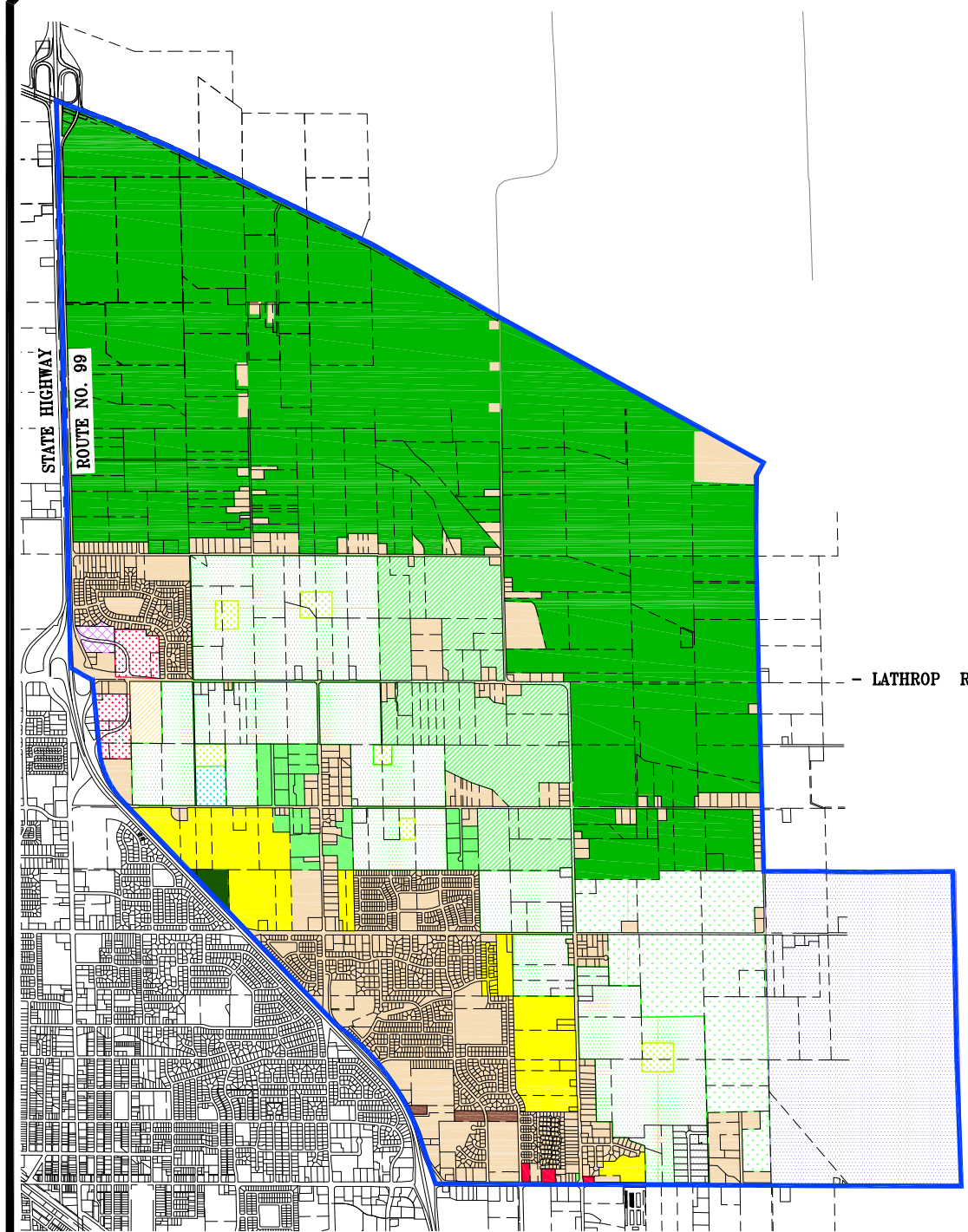


PLATE A-1.4

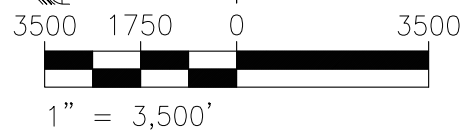
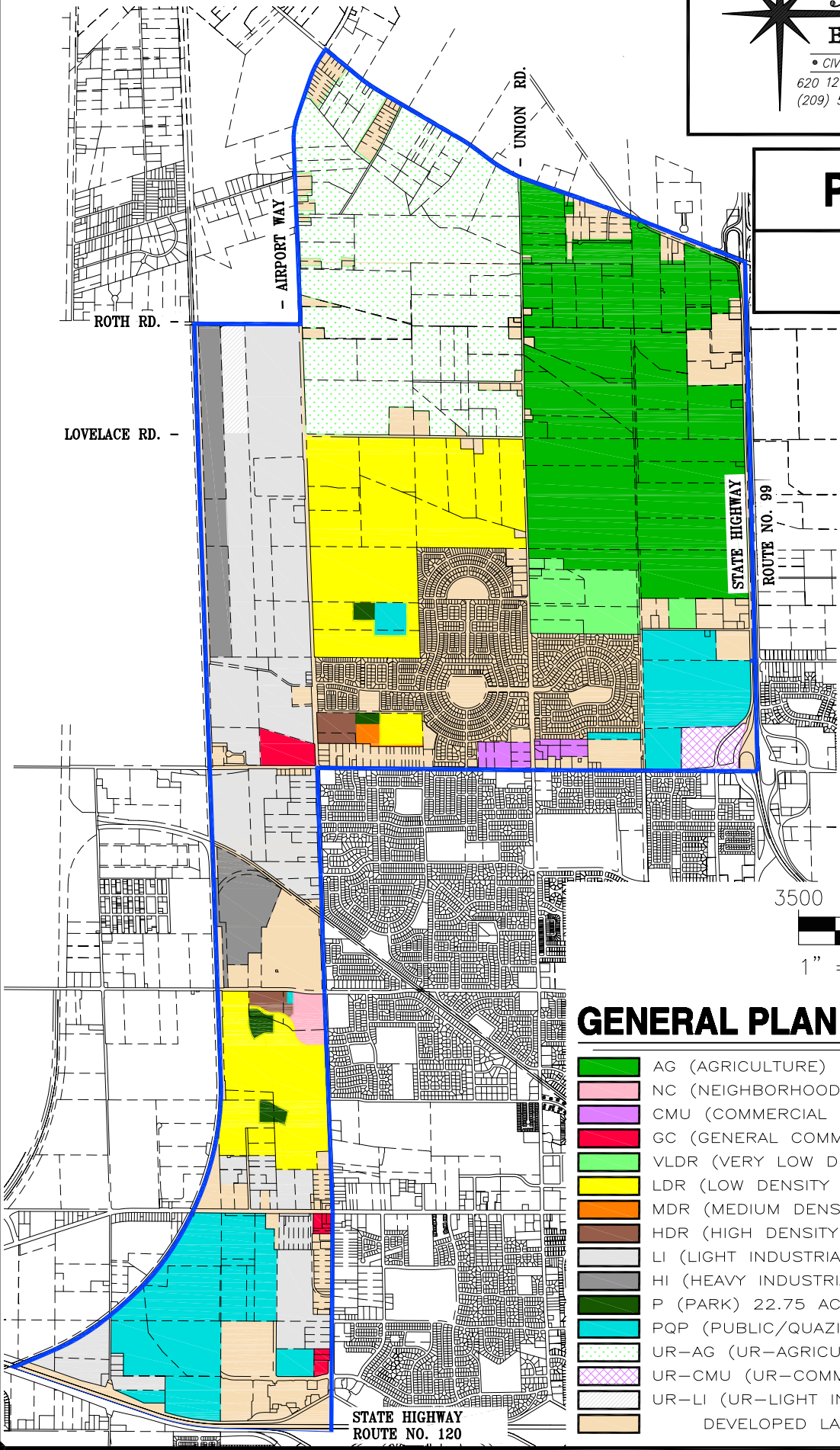
ZONE 4



















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 • CIVIL ENGINEERING • SURVEYING • PLANNING •
 620 12th Street Modesto, CA 95354
 (209) 524-3525 Phone (209) 524-3526 Fax

PLATE A-1.5

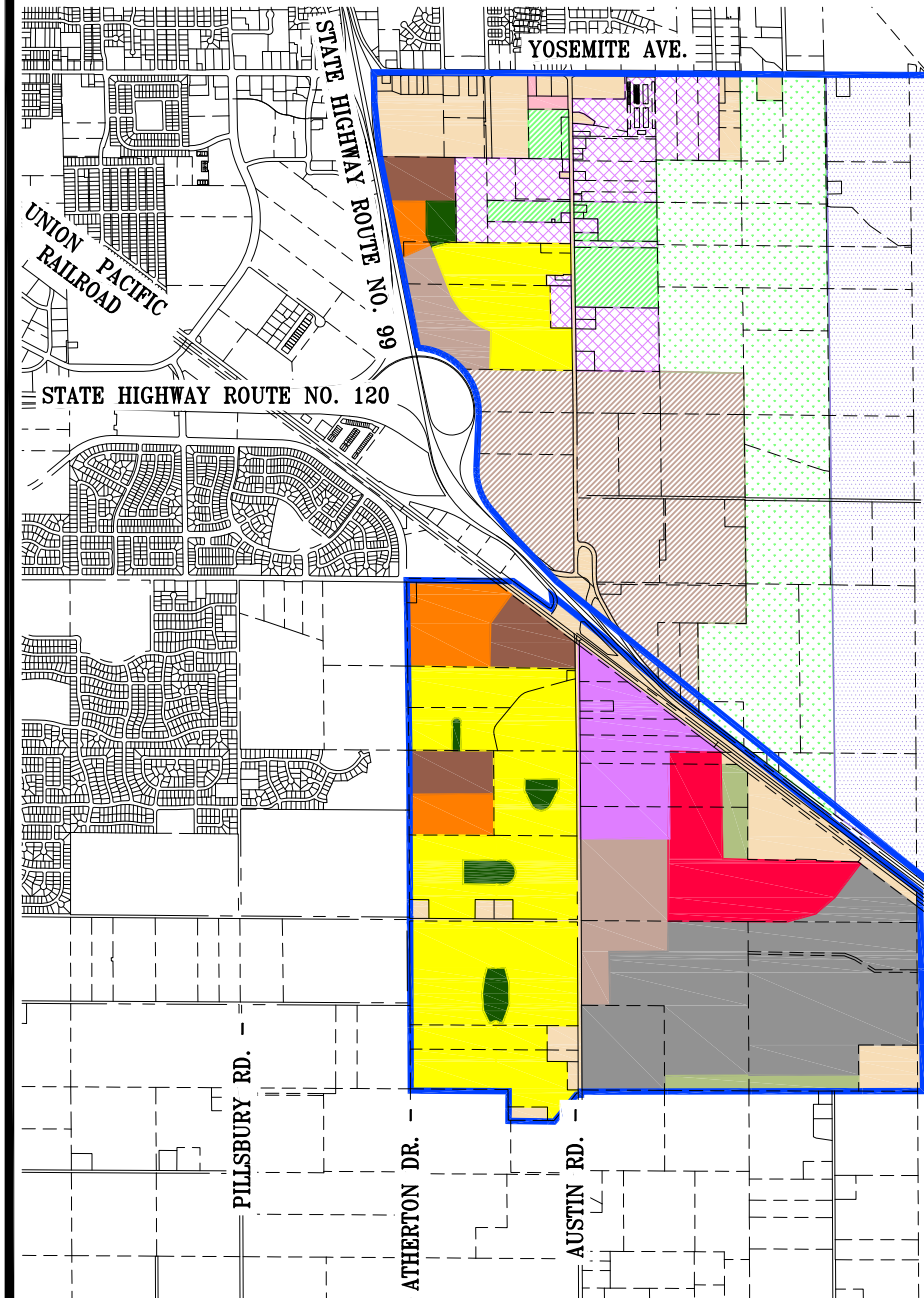
ZONE 5



GENERAL PLAN DESIGNATION

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	GC (GENERAL COMMERCIAL) 31.15 AC
	VLDR (VERY LOW DENSITY RES.) 107.87 AC
	LDR (LOW DENSITY RES.) 642.49 AC
	MDR (MEDIUM DENSITY RES.) 26.10 AC
	HDR (HIGH DENSITY RES.) 26.10 AC
	LI (LIGHT INDUSTRIAL) 723.74 AC
	HI (HEAVY INDUSTRIAL) 162.56 AC
	P (PARK) 22.75 AC
	PQP (PUBLIC/QUAZI-PUBLIC) 455.03 AC
	UR-AG (UR-AGRICULTURE) 864.55 AC
	UR-CMU (UR-COMMERCIAL MIXED USE) 33.70 AC
	UR-LI (UR-LIGHT INDUSTRIAL) 35.00 AC
	DEVELOPED LAND

STATE HIGHWAY
 ROUTE NO. 120



GENERAL PLAN DESIGNATION

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	GC (GENERAL COMMERCIAL) 90.21 AC
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	MDR (MEDIUM DENSITY RES.) 70.71 AC
	HDR (HIGH DENSITY RES.) 62.96 AC
	BIP (BUSINESS INDUSTRIAL PARK) 96.56 AC
	HI (HEAVY INDUSTRIAL) 321.73 AC
	OS (OPEN SPACE) 30.42 AC
	P (PARK) 27.20 AC
	UR (URBAN-RESERVE) 422.70 AC
	UR-AG (UR-AGRICULTURE) 464.55 AC
	UR-CMU (UR-COMMERCIAL MIXED USE) 165.93 AC
	UR-VLDR (UR-VERY LOW DENSITY RES.) 64.64 AC
	UR-BIP (UR-BUSINESS IND. PARK) 343.56 AC
	DEVELOPED LAND

- WOODWARD AVE.

- RAYMUS EXPRESSWAY

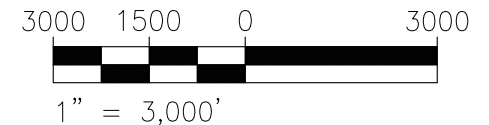


PLATE A-1.6

ZONE 6

Appendix E
Transportation Data

TRAFFIC ZONES

-  ZONE 1
-  ZONE 2
-  ZONE 3
-  ZONE 4
-  ZONE 5
-  ZONE 6



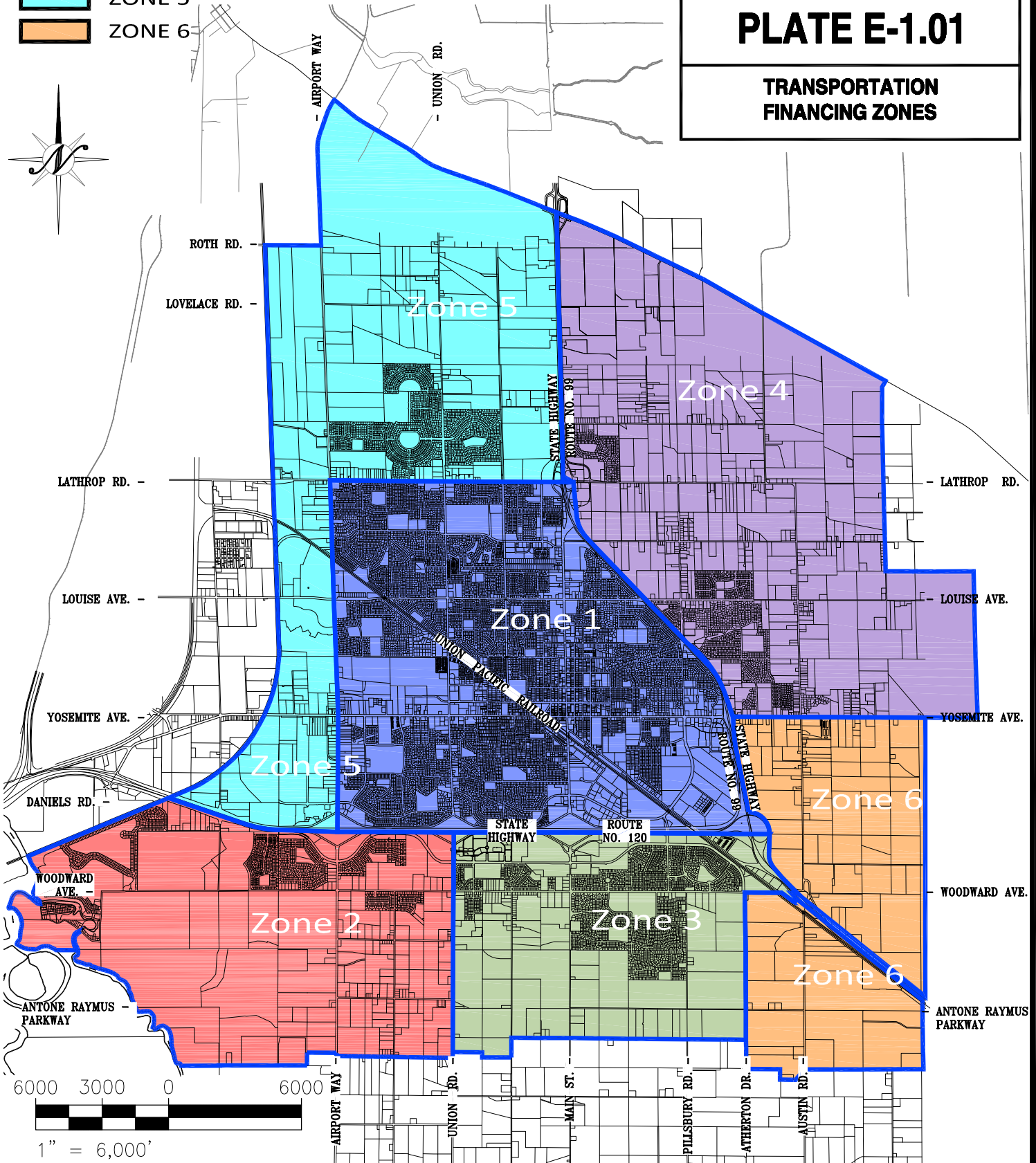
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PLATE E-1.01

TRANSPORTATION FINANCING ZONES



ROADWAY SEGMENTS

AP1.1 S AIRPORT WY - FRENCH CAMP RD TO E ROTH RD
 AP1.2 S AIRPORT WY - FRENCH CAMP RD TO E ROTH RD
 AP2 S AIRPORT WY - E ROTH RD TO LOVELACE RD
 AP3.1 S AIRPORT WY - E LOVELACE RD TO DAISYWOOD DR
 AP3.2 S AIRPORT WY - E LOVELACE RD TO DAISYWOOD DR
 AP4 S AIRPORT WY - DAISYWOOD DR TO LATHROP RD
 AP5 S AIRPORT WY - LATHROP RD TO NORTHGATE DR
 AP6 S AIRPORT WY - NORTHGATE DR TO UPRR CROSSING
 AP7 S AIRPORT WY - UPRR CROSSING TO LOUISE AVE
 AP8 S AIRPORT WY - LOUISE AVE TO CROM ST
 AP9 S AIRPORT WY - CROM ST TO W YOSEMITE AVE
 AP10 S AIRPORT WY - W YOSEMITE AVE TO WAWONA ST
 AP14.1 S AIRPORT WY - W WOODWARD AVE TO ANTONE RAYMUS PKWY
 AP14.2 S AIRPORT WY - W WOODWARD AVE TO ANTONE RAYMUS PKWY
 AT1 E ATHERTON DR - W WOODWARD AVE TO MCKINLEY AVE
 AT3 E ATHERTON DR - MCKINLEY AVE TO 825' E OF HEARTHSONG DR
 AT4.1 E ATHERTON DR - 750' W OF OLEANER DR TO S UNION RD
 AT4.2 E ATHERTON DR - 750' W OF OLEANER DR TO S UNION RD
 AT5.1 W ATHERTON DR - E WOODWARD AVE TO ANTONE RAYMUS PKWY
 AT5.2 W ATHERTON DR - E WOODWARD AVE TO ANTONE RAYMUS PKWY
 AU1a AUSTIN RD - E YOSEMITE AVE TO 2650' S OF E YOSEMITE AVE
 AU1b.1 AUSTIN RD - 2650' S OF E YOSEMITE AVE TO GRAVES RD
 AU1b.2 AUSTIN RD - 2650' S OF E YOSEMITE AVE TO GRAVES RD
 AU2 AUSTIN RD - GRAVES DR TO UPRR CROSSING
 AU4 AUSTIN RD - UPRR CROSSING TO ANTONE RAYMUS PKWY
 DAN1 DANIELS ST - MCKINLEY AVE TO EX DANIELS ST
 LA1 W LATHROP RD - 1800' W OF S AIRPORT WY TO AIRPORT WY
 LA2 E LATHROP RD - AIRPORT WY TO LONDON AVE
 LA3 E LATHROP RD - ARROWSMITH DR TO CRESTWOOD AVE
 LA4.1 E LATHROP RD - S FRONTAGE RD TO COTTAGE AVE
 LA4.2 E LATHROP RD - S FRONTAGE RD TO COTTAGE AVE
 LO1 W LOUISE AVE - UPRR CROSSING TO S AIRPORT WY
 LO2 W LOUISE AVE - 600' E OF MAIN ST TO GARDEN GATE DR
 LO3 W LOUISE AVE - HWY 99 TO FELICE WY
 LO4 W LOUISE AVE - 700' E OF PESTANA AVE TO AUSTIN RD
 LOV1.1 E LOVELACE RD - S AIRPORT WY TO S UNION RD
 LOV1.2 E LOVELACE RD - S AIRPORT WY TO S UNION RD
 LOV2.1 E LOVELACE RD - S UNION RD TO HWY 99
 LOV2.2 E LOVELACE RD - S UNION RD TO HWY 99
 LOV2.3 E LOVELACE RD - S UNION RD TO HWY 99
 MA2.1 S MAIN ST - E WOODWARD AVE TO ANTONE RAYMUS PKWY
 MA2.2 S MAIN ST - E WOODWARD AVE TO ANTONE RAYMUS PKWY
 MC1 MCKINLEY AVE - UPRR CROSSING TO SR 120
 MC3 MCKINLEY AVE - W ATHERTON DR TO W WOODWARD AVE
 MC4 MCKINLEY AVE - W WOODWARD AVE TO ANTONE RAYMUS PKWY
 MO1 MOFFAT BOULEVARD - LAURENWOOD RD TO SR 120 EXIT RAMP
 RE1.1 ANTONE RAYMUS PKWY - W WOODWARD AVE TO S AIRPORT WY
 RE1.2 ANTONE RAYMUS PKWY - W WOODWARD AVE TO S AIRPORT WY
 RE2.1 ANTONE RAYMUS PKWY - S AIRPORT WY TO S UNION RD
 RE2.2 ANTONE RAYMUS PKWY - S AIRPORT WY TO S UNION RD
 RE3a ANTONE RAYMUS PKWY - S UNION RD TO 900' E OF TINNIN RD
 RE3b ANTONE RAYMUS PKWY - 900'E OF TINNIN RD TO S MAIN ST
 RE4.1 ANTONE RAYMUS PKWY - S MAIN ST TO E ATHERTON DR
 RE4.2 ANTONE RAYMUS PKWY - S MAIN ST TO E ATHERTON DR
 RE4.3 ANTONE RAYMUS PKWY - S MAIN ST TO E ATHERTON DR
 RE5 ANTONE RAYMUS PKWY - E ATHERTON DR TO AUSTIN RD
 RE6.1 ANTONE RAYMUS PKWY - AUSTIN RD TO HWY 99
 RE6.2 ANTONE RAYMUS PKWY - AUSTIN RD TO HWY 99
 RE7 ANTONE RAYMUS PKWY - HWY 99 TO UNKNOWN RD
 RO1 E ROTH RD - CITY LIMITS TO S AIRPORT WY
 UN1.1 S UNION RD - FRENCH CAMP RD TO E LOVELACE RD
 UN1.2 S UNION RD - FRENCH CAMP RD TO E LOVELACE RD
 UN2 S UNION RD - LOVELACE RD TO SHADY PINES ST
 UN3.1 S UNION RD - W WOODWARD AVE TO ANTONE RAYMUS PKWY
 UN3.2 S UNION RD - W WOODWARD AVE TO ANTONE RAYMUS PKWY
 WO1.1 W WOODWARD AVE - APPLICELLA CT TO W ATHERTON DR
 WO1.2 W WOODWARD AVE - APPLICELLA CT TO W ATHERTON DR
 WO2 W WOODWARD AVE - MCKINLEY AVE TO S AIRPORT WY
 WO3.1 W WOODWARD AVE - S AIRPORT WY TO S UNION RD
 WO3.2 W WOODWARD AVE - S AIRPORT WY TO S UNION RD
 WO4 W WOODWARD AVE - S UNION RD TO 400' E OF PAGOLA AVE
 WO5 W WOODWARD AVE - 400' E OF PAGOLA AVE TO TINNIN RD
 WO6 W WOODWARD AVE - TINNIN RD TO 1300' E OF TINNIN RD
 WO7 W WOODWARD AVE - 1300' E OF TINNIN RD TO S MAIN ST
 WO8 E WOODWARD AVE - PILLSBURY RD TO E ATHERTON AVE
 WO9.1 E WOODWARD AVE - E ATHERTON AVE TO AUSTIN RD
 WO9.2 E WOODWARD AVE - E ATHERTON AVE TO AUSTIN RD
 WO10 W WOODWARD AVE - W ATHERTON AVE TO MCKINLEY AVE
 YO1 E YOSEMITE AVE - HWY 99 TO AUSTIN RD
 YO2.1 E YOSEMITE AVE - AUSTIN RD TO COMCONEX RD
 YO2.2 E YOSEMITE AVE - AUSTIN RD TO COMCONEX RD

INTERSECTIONS

INT1 S AIRPORT WY & E ROTH RD
 INT2 S AIRPORT WY & E FISK RD
 INT3 S AIRPORT WY & E LOVELACE RD
 INT4 S AIRPORT WY & DAISYWOOD DR
 INT5 S AIRPORT WY & LATHROP RD
 INT6 S AIRPORT WY & LOUISE AVE
 INT7 S AIRPORT WY & CROM ST
 INT8 S AIRPORT WY & W YOSEMITE AVE
 INT9 S AIRPORT WY & WAWONA ST
 INT10 S AIRPORT WY & DANIELS ST
 INT11 S AIRPORT WY & W WOODWARD AVE
 INT12 MCKINLEY AVE & W ATHERTON RD
 INT13 E WOODWARD AVE & E ATHERTON DR
 INT14A W ATHERTON DR & S UNION RD
 INT14B W ATHERTON DR & S UNION RD
 INT15 ATHERTON DR & S MAIN ST
 INT16 W WOODWARD AVE & W ATHERTON DR
 INT17 AUSTIN RD & E GRAVES RD
 INT18 S MAIN ST & MISSION/INDUSTRIAL DR
 INT19 AUSTIN RD & ANTONE RAYMUS PKWY
 INT20 W LATHROP RD & COTTAGE AVE
 INT21 W LOUISE AVE & GARDEN GATE DR
 INT22 W LOUISE AVE & SOUZA BLVD
 INT23 W LOUISE AVE & FELICE WAY
 INT24 E WOODWARD AVE & S MAIN ST
 INT25 W WOODWARD AVE & MCKINLEY AVE
 INT26 S AIRPORT WY & ANTONE RAYMUS PKWY
 INT27 S UNION RD & ANTONE RAYMUS PKWY
 INT28 S MAIN ST & ANTONE RAYMUS PKWY
 INT29 S MAIN ST & SPRINGFIELD RD
 INT30 E ATHERTON DR & ANTONE RAYMUS PKWY
 INT31 S UNION RD & E LOVELACE RD
 INT32 S AIRPORT WY & GENEVA WY
 INT33 W WOODWARD AVE & S UNION RD
 INT34 E ATHERTON DR & VAN RYN AVE
 INT35 E WOODWARD AVE & PILLSBURY AVE
 INT36 MOFFAT DR & LAURENWOOD RD
 INT37 INDUSTRIAL PARK DR & VAN RYN AVE
 INT38 MCKINLEY AVE & DANIELS ST
 INT40 AUSTIN RD & E YOSEMITE AVE
 INT41 S AIRPORT WY & W ATHERTON DR
 INT42 E WOODWARD AVE & LAURENWOOD RD
 INT43 E WOODWARD AVE & AUSTIN RD
 INT44 W YOSEMITE AVE & MILO CANDINI DR
 INT45 S AIRPORT WY & W CENTER ST
 INT46 W YOSEMITE AVE & FISHBACK RD
 INT47 W YOSEMITE AVE & ST. DOMINICS DR
 INT48 W YOSEMITE AVE & WINTERS DR
 INT49 E LATHROP RD & S. FRONTAGE DR
 INT50 E LATHROP RD & PUEBLO DR
 INT51 E WOODWARD AVE & BUENA VISTA DR
 INT52 E WOODWARD AVE & VAN RYN AVE
 INT54 W WOODWARD AVE & UNKNOWN ST
 INT55 S AIRPORT WAY & E PEACH RD
 INT56 S UNION RD & E PEACH RD
 INT57 ANTONE RAYMUS PKWY & E PEACH RD
 INT58 ANTONE RAYMUS PKWY & TINNIN RD
 INT59 S MAIN ST & TANNEHILL DR
 INT60 ANTONE RAYMUS PKWY & PILLSBURY RD



INTERCHANGES

XCHG-1 MCKINLEY AVE & SR 120
 XCHG-2 AIRPORT WAY & SR 120
 XCHG-3 UNION RD & SR 120
 XCHG-4 MAIN ST & SR 120
 XCHG-5 LOUISE AVE & SR 120 OVERCROSSING
 XCHG-6 COTTAGE AVE & SR 120 OVERCROSSING

PLATE E-1.02

PROJECT LIST

ABBREVIATIONS

- AP - AIRPORT WAY
- AT - ATHERTON DRIVE
- AU - AUSTIN ROAD
- DAN - DANIELS STREET
- LA - LATHROP ROAD
- LO - LOUISE AVENUE
- LOV - LOVELACE ROAD
- MA - MAIN STREET
- MC - MCKINLEY AVENUE
- MO - MOFFAT BOULEVARD
- RE - ANTONE RAYMUS PARKWAY
- RO - ROTH ROAD
- UN - UNION ROAD
- WO - WOODWARD AVENUE
- YO - YOSEMITE AVENUE

ROADWAY SEGMENT TYPES

- 2 LANE ROAD
- 2 LANE ROAD - ANTONE RAYMUS PARKWAY
- 4 LANE ROAD
- 4 LANE ROAD - AIRPORT ROAD
- 6 LANE ROAD



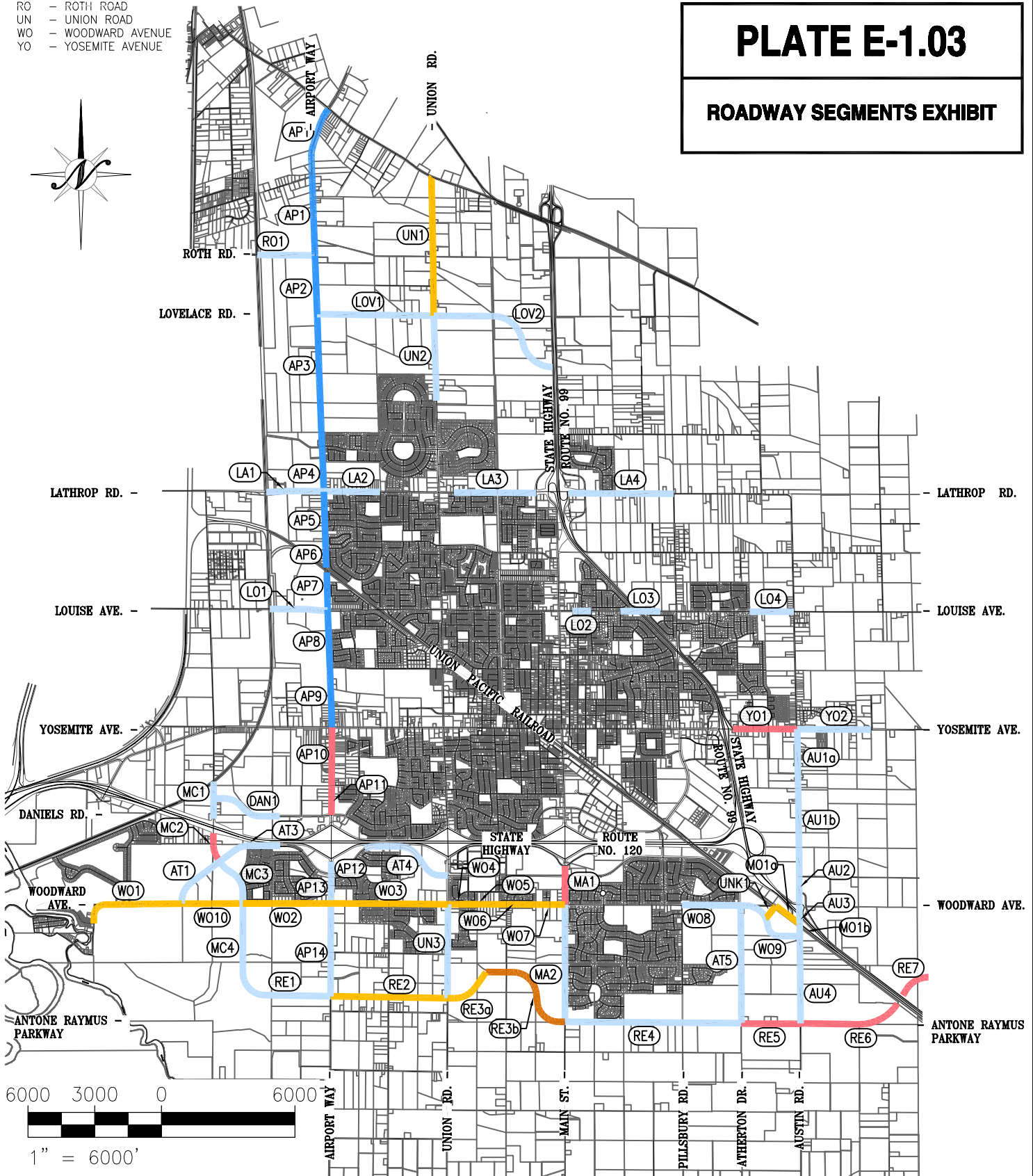
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PLATE E-1.03

ROADWAY SEGMENTS EXHIBIT



INTERSECTION TYPES

-  NEW ROUNDABOUT
-  NEW SIGNAL
-  MODIFY SIGNAL
-  RE-STRIPING ONLY/LOOPS



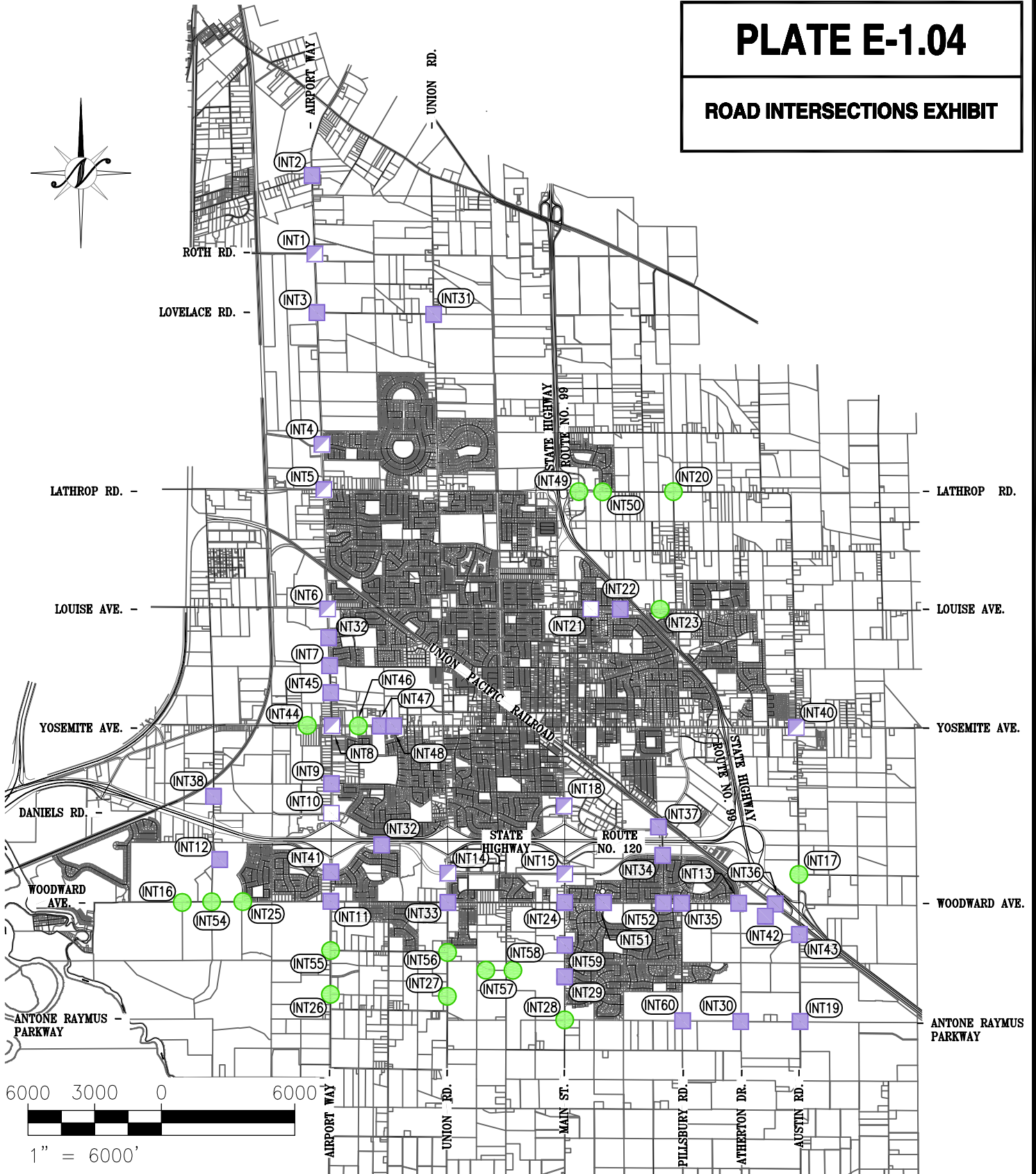
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PLATE E-1.04

ROAD INTERSECTIONS EXHIBIT





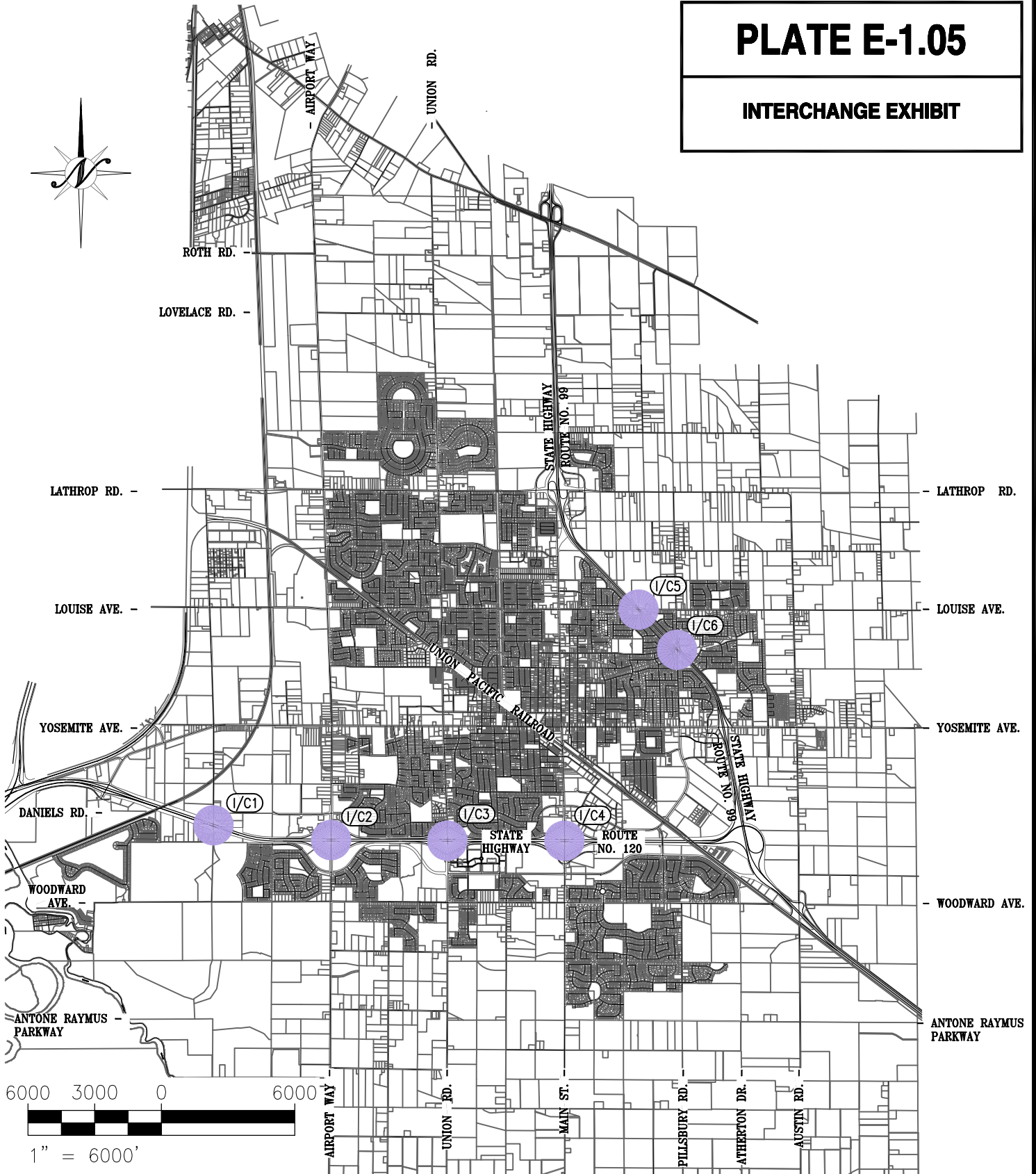
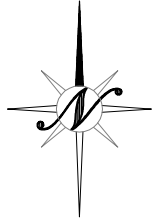
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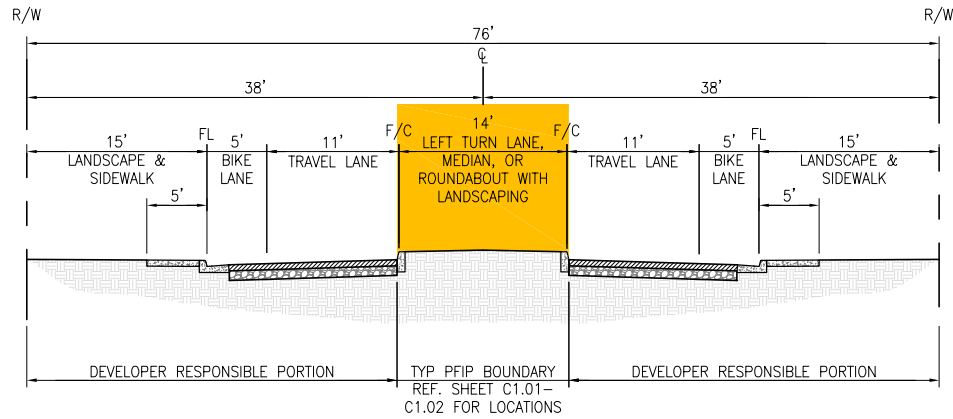
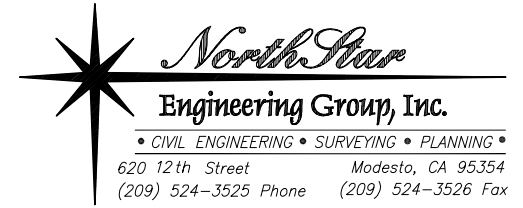
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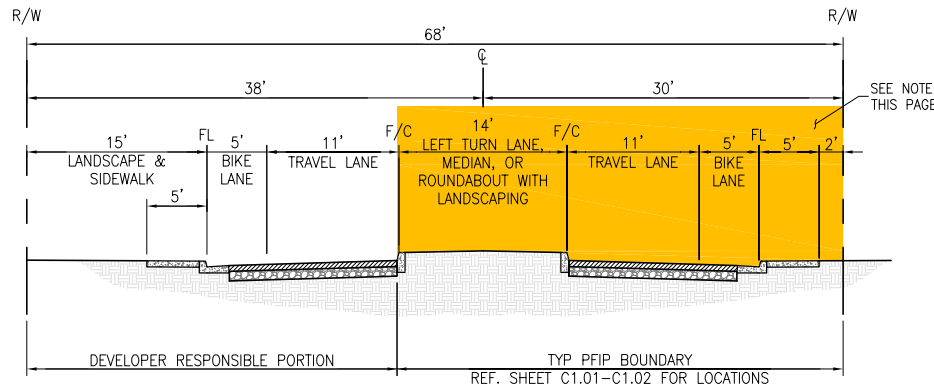
PLATE E-1.05

INTERCHANGE EXHIBIT





A1 **2 LANE STREET SECTION**
 (TI-7, ASSUMED R VALUE=30, 6"AC/8"AB)
 NTS



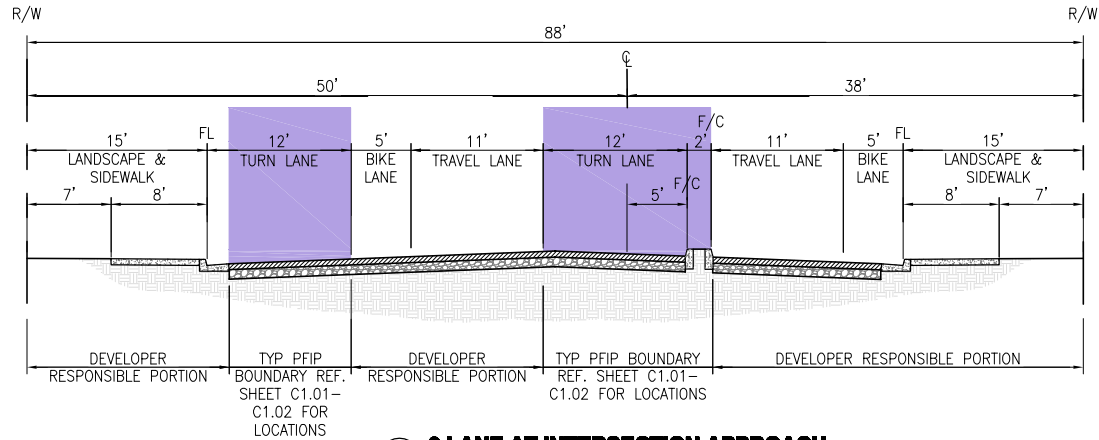
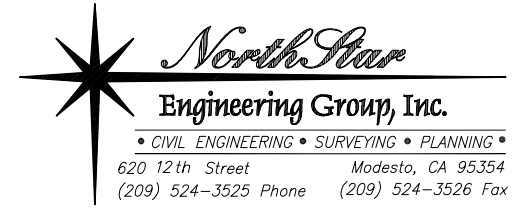
A2 **MODIFIED 2 LANE STREET SECTION**
 (TI-7, ASSUMED R VALUE=30, 6"AC/8"AB)
 NTS

NOTE

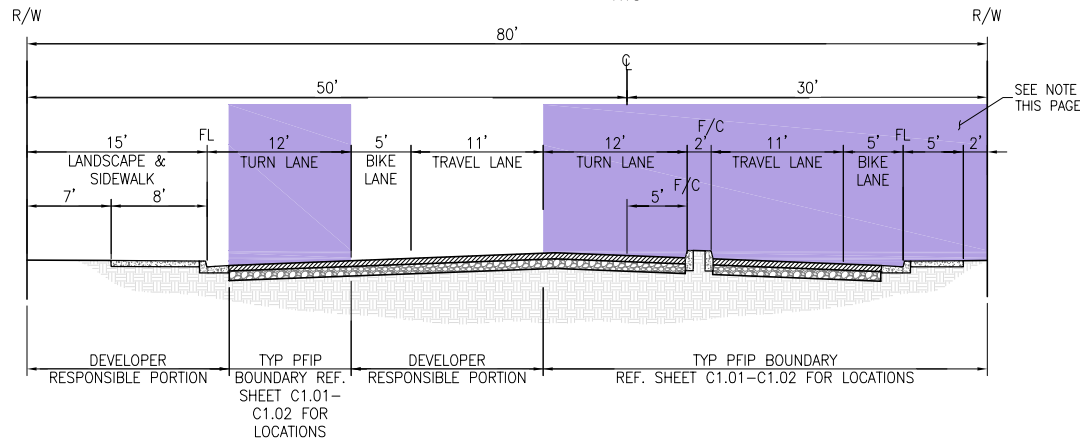
THE PFIIP LIMITS OF WORK ARE INCREASED IN AREAS WHERE FUTURE IMPROVEMENTS ARE NOT ANTICIPATED TO BE CONSTRUCTED AS PART OF FUTURE DEVELOPMENT PROJECTS. IN THESE SPECIFIC AREAS A REDUCED SIDEWALK AND PARKSTRIP IS TO BE UTILIZED AS SHOWN IN THIS CROSS SECTION. THESE ADDITIONAL IMPROVEMENTS MAY BE REQUIRED ON ONE SIDE OR BOTH SIDES OF THE SECTION TO COMPLETE THE PFIIP IMPROVEMENTS. THESE AREAS ARE DENOTED ON THE PLANS. THE IMPROVEMENT COSTS ARE ACCOUNTED FOR IN THE PFIIP FEE.

PLATE E-2.01

2 LANE STREET CROSS-SECTIONS



B1 2 LANE AT INTERSECTION APPROACH
 (TI-7, ASSUMED R VALUE=30, 6"AC/8"AB)
 NTS



B2 MODIFIED 2 LANE AT INTERSECTION APPROACH
 (TI-7, ASSUMED R VALUE=30, 6"AC/8"AB)
 NTS

NOTE

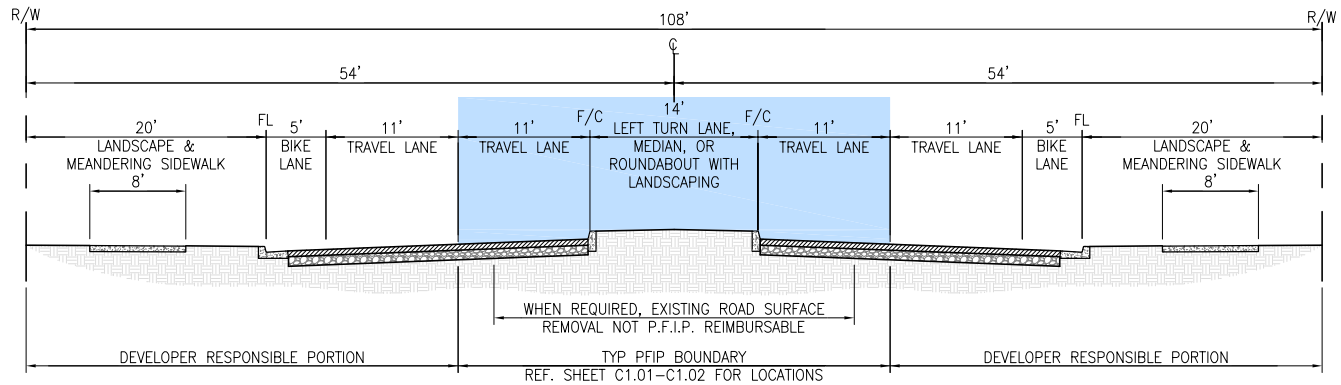
THE PFIP LIMITS OF WORK ARE INCREASED IN AREAS WHERE FUTURE IMPROVEMENTS ARE NOT ANTICIPATED TO BE CONSTRUCTED AS PART OF FUTURE DEVELOPMENT PROJECTS. IN THESE SPECIFIC AREAS A REDUCED SIDEWALK AND PARKSTRIP IS TO BE UTILIZED AS SHOWN IN THIS CROSS SECTION. THESE ADDITIONAL IMPROVEMENTS MAY BE REQUIRED ON ONE SIDE OR BOTH SIDES OF THE SECTION TO COMPLETE THE PFIP IMPROVEMENTS. THESE AREAS ARE DENOTED ON THE PLANS. THE IMPROVEMENT COSTS ARE ACCOUNTED FOR IN THE PFIP FEE.

PLATE E-2.02

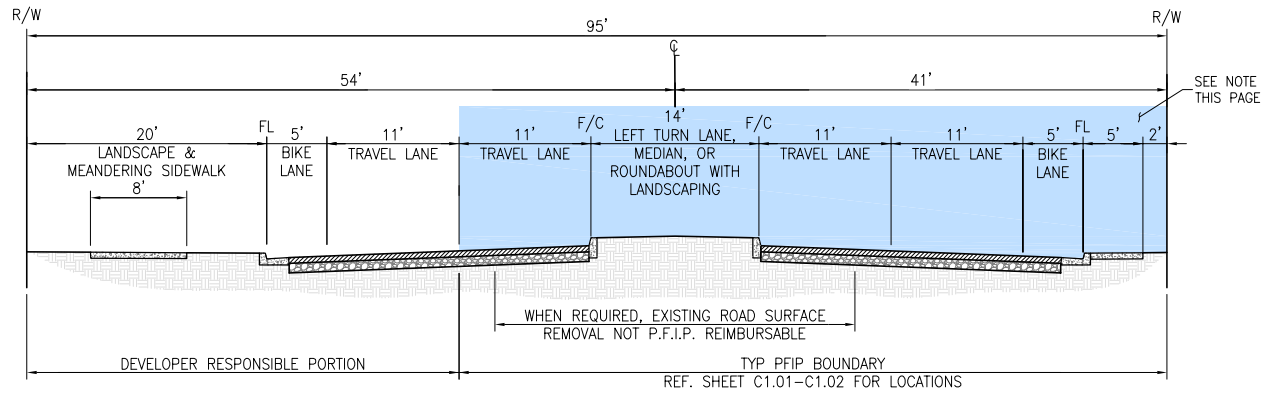
**2 LANE STREET CROSS-SECTIONS
 AT INTERSECTIONS
 SINGLE TURN LANE**



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C1 4 LANE STREET SECTION
 (TI-9, ASSUMED R VALUE=30, 7"AC/13"AB)
 NTS



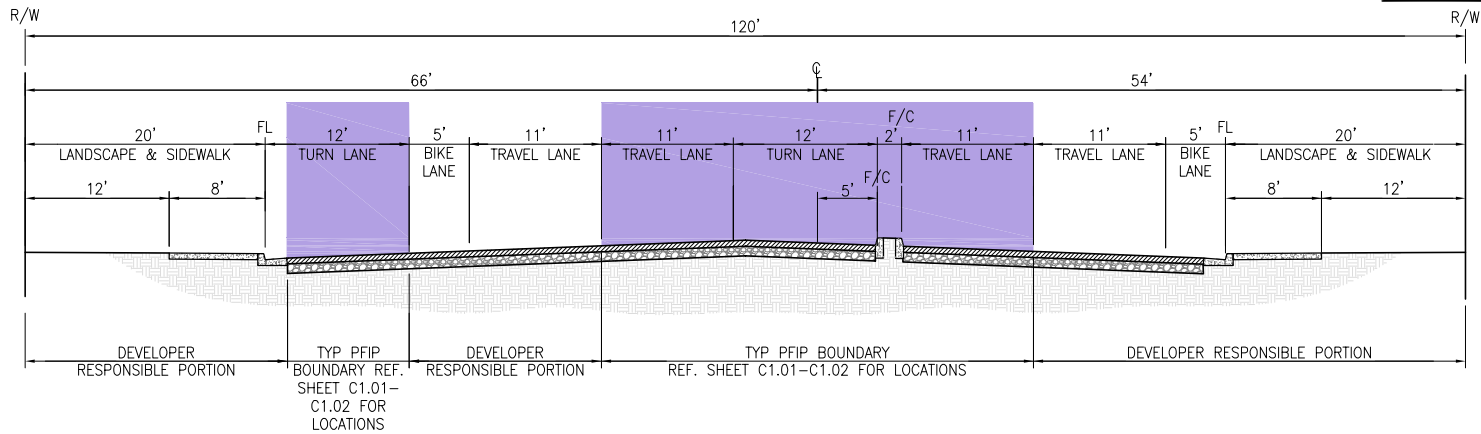
C2 MODIFIED 4 LANE STREET SECTION
 (TI-9, ASSUMED R VALUE=30, 7"AC/13"AB)
 NTS

NOTE

THE PPIP LIMITS OF WORK ARE INCREASED IN AREAS WHERE FUTURE IMPROVEMENTS ARE NOT ANTICIPATED TO BE CONSTRUCTED AS PART OF FUTURE DEVELOPMENT PROJECTS. IN THESE SPECIFIC AREAS A REDUCED SIDEWALK AND PARKSTRIP IS TO BE UTILIZED AS SHOWN IN THIS CROSS SECTION. THESE ADDITIONAL IMPROVEMENTS MAY BE REQUIRED ON ONE SIDE OR BOTH SIDES OF THE SECTION TO COMPLETE THE PPIP IMPROVEMENTS. THESE AREAS ARE DENOTED ON THE PLANS. THE IMPROVEMENT COSTS ARE ACCOUNTED FOR IN THE PPIP FEE.

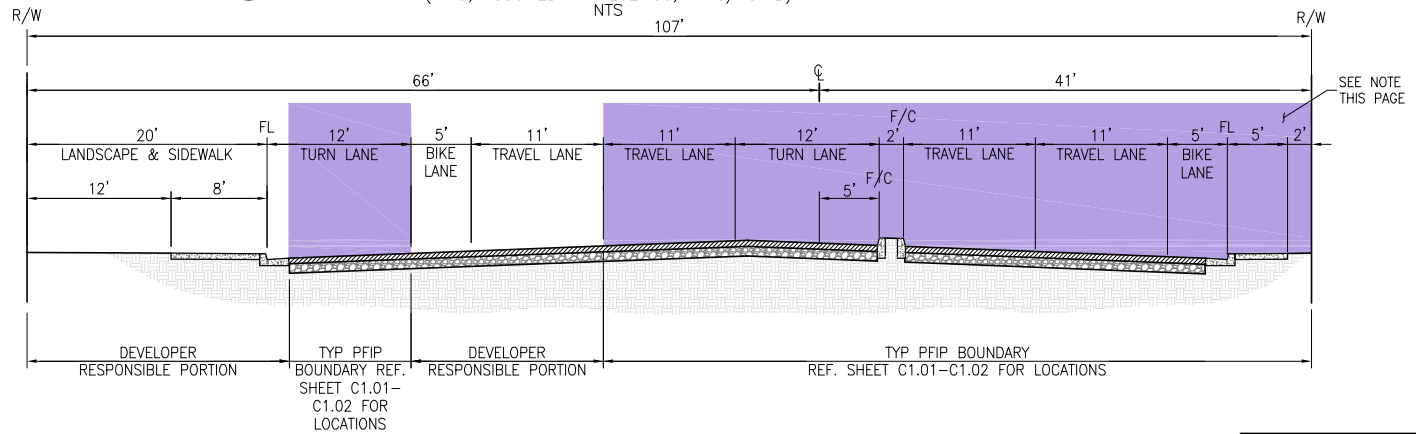
PLATE E-2.03

4 LANE STREET CROSS-SECTIONS



D1 4 LANE AT INTERSECTION APPROACH - SINGLE LEFT TURN LANE

(TI-9, ASSUMED R VALUE=30, 7"AC/13"AB)
 NTS



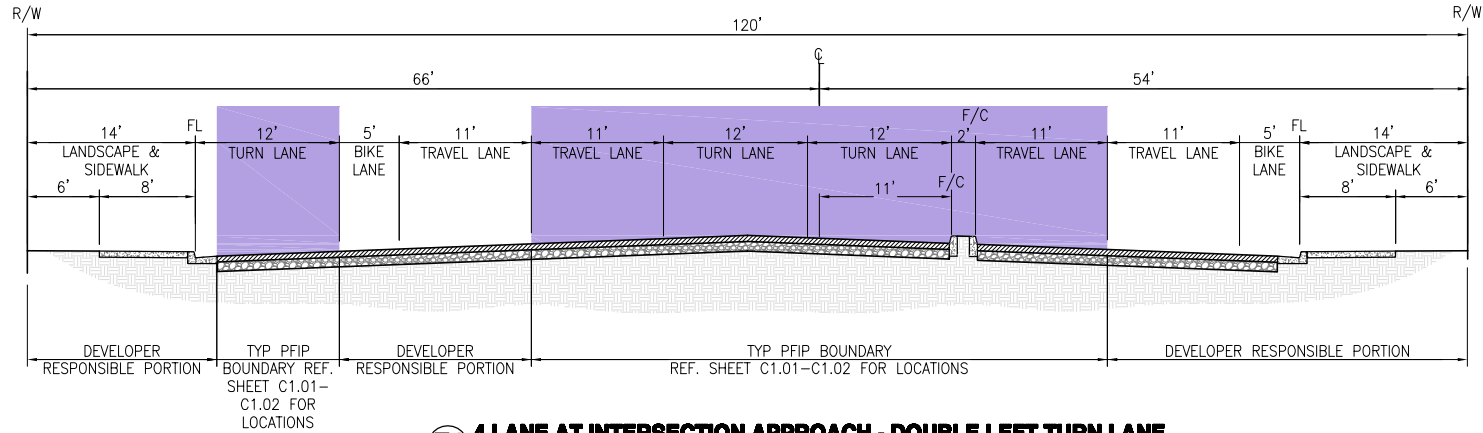
D2 MODIFIED 4 LANE AT INTERSECTION APPROACH - SINGLE LEFT TURN LANE

(TI-9, ASSUMED R VALUE=30, 7"AC/13"AB)
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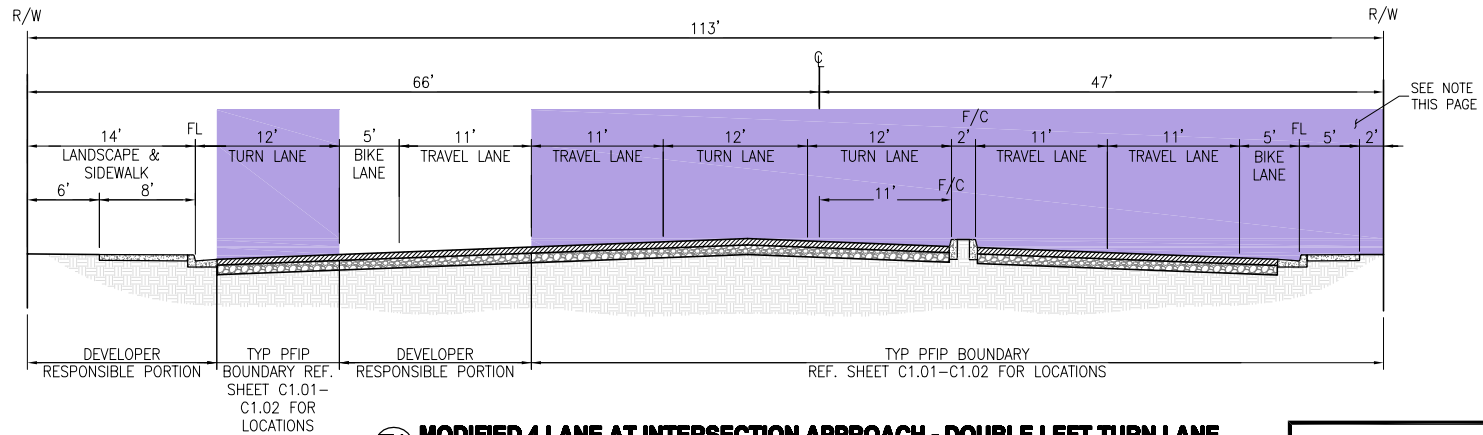
NOTE

THE PFIIP LIMITS OF WORK ARE INCREASED IN AREAS WHERE FUTURE IMPROVEMENTS ARE NOT ANTICIPATED TO BE CONSTRUCTED AS PART OF FUTURE DEVELOPMENT PROJECTS. IN THESE SPECIFIC AREAS A REDUCED SIDEWALK AND PARKSTRIP IS TO BE UTILIZED AS SHOWN IN THIS CROSS SECTION. THESE ADDITIONAL IMPROVEMENTS MAY BE REQUIRED ON ONE SIDE OR BOTH SIDES OF THE SECTION TO COMPLETE THE PFIIP IMPROVEMENTS. THESE AREAS ARE DENOTED ON THE PLANS. THE IMPROVEMENT COSTS ARE ACCOUNTED FOR IN THE PFIIP FEE.

PLATE E-2.04
4 LANE STREET CROSS-SECTIONS
AT INTERSECTIONS
SINGLE TURN LANE



E1 4 LANE AT INTERSECTION APPROACH - DOUBLE LEFT TURN LANE
 (TI-9, ASSUMED R VALUE=30, 7"AC/13"AB)
 NTS

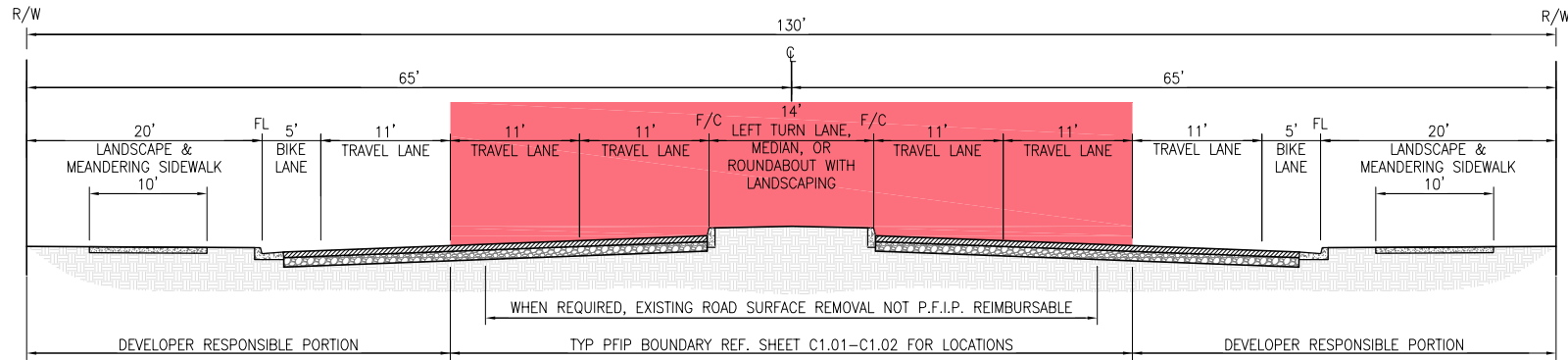


E2 MODIFIED 4 LANE AT INTERSECTION APPROACH - DOUBLE LEFT TURN LANE
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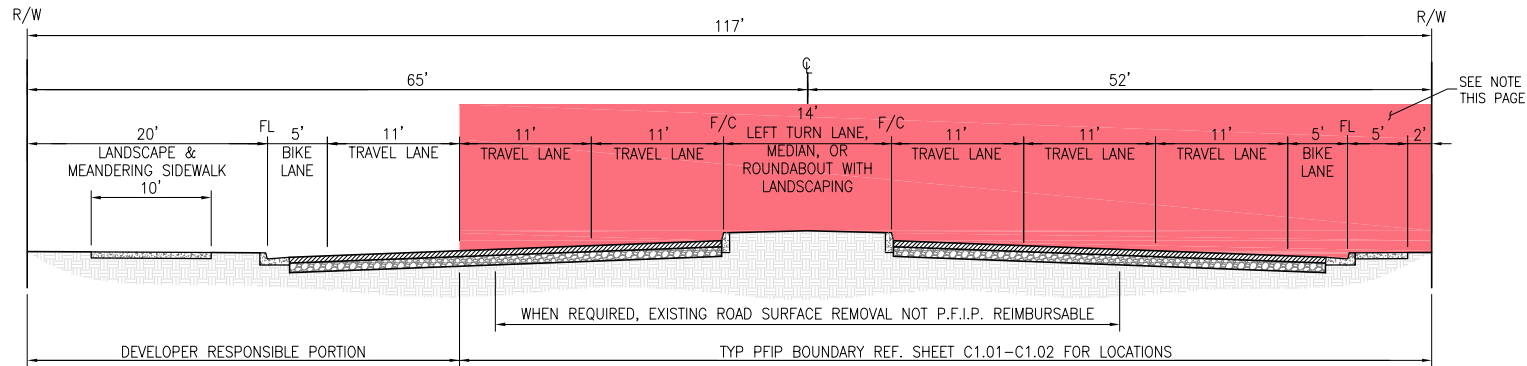
NOTE

THE PFIIP LIMITS OF WORK ARE INCREASED IN AREAS WHERE FUTURE IMPROVEMENTS ARE NOT ANTICIPATED TO BE CONSTRUCTED AS PART OF FUTURE DEVELOPMENT PROJECTS. IN THESE SPECIFIC AREAS A REDUCED SIDEWALK AND PARKSTRIP IS TO BE UTILIZED AS SHOWN IN THIS CROSS SECTION. THESE ADDITIONAL IMPROVEMENTS MAY BE REQUIRED ON ONE SIDE OR BOTH SIDES OF THE SECTION TO COMPLETE THE PFIIP IMPROVEMENTS. THESE AREAS ARE DENOTED ON THE PLANS. THE IMPROVEMENT COSTS ARE ACCOUNTED FOR IN THE PFIIP FEE.

PLATE E-2.05
4 LANE STREET CROSS-SECTIONS
AT INTERSECTIONS
DOUBLE TURN LANE



F1 6 LANE STREET SECTION
 (TI-11, ASSUMED R VALUE=30, 9"AC/16"AB)
 NTS



F2 MODIFIED 6 LANE STREET SECTION
 (TI-11, ASSUMED R VALUE=30, 9"AC/16"AB)
 NTS

NOTE

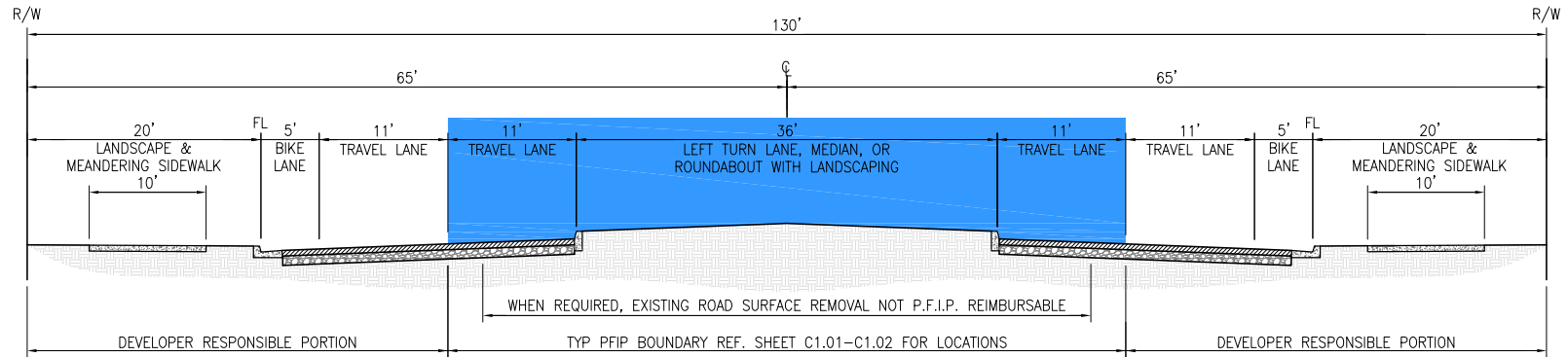
THE PFIP LIMITS OF WORK ARE INCREASED IN AREAS WHERE FUTURE IMPROVEMENTS ARE NOT ANTICIPATED TO BE CONSTRUCTED AS PART OF FUTURE DEVELOPMENT PROJECTS. IN THESE SPECIFIC AREAS A REDUCED SIDEWALK AND PARKSTRIP IS TO BE UTILIZED AS SHOWN IN THIS CROSS SECTION. THESE ADDITIONAL IMPROVEMENTS MAY BE REQUIRED ON ONE SIDE OR BOTH SIDES OF THE SECTION TO COMPLETE THE PFIP IMPROVEMENTS. THESE AREAS ARE DENOTED ON THE PLANS. THE IMPROVEMENT COSTS ARE ACCOUNTED FOR IN THE PFIP FEE.

PLATE E-2.06

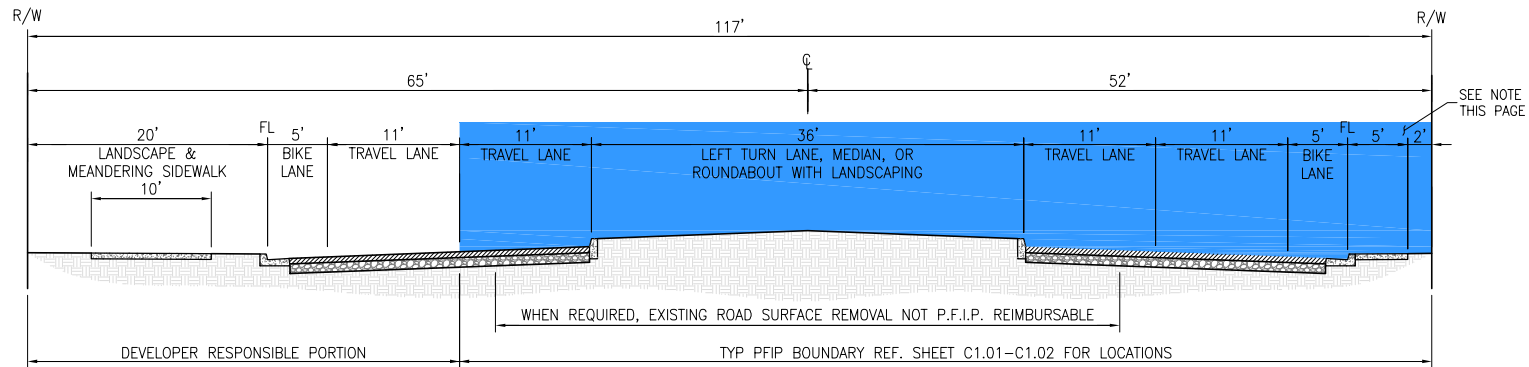
6 LANE STREET CROSS-SECTIONS



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F3 AIRPORT WAY STREET SECTION, NORTH OF YOSEMITE
 (TI-11, ASSUMED R VALUE=30, 9"AC/16"AB)
 NTS



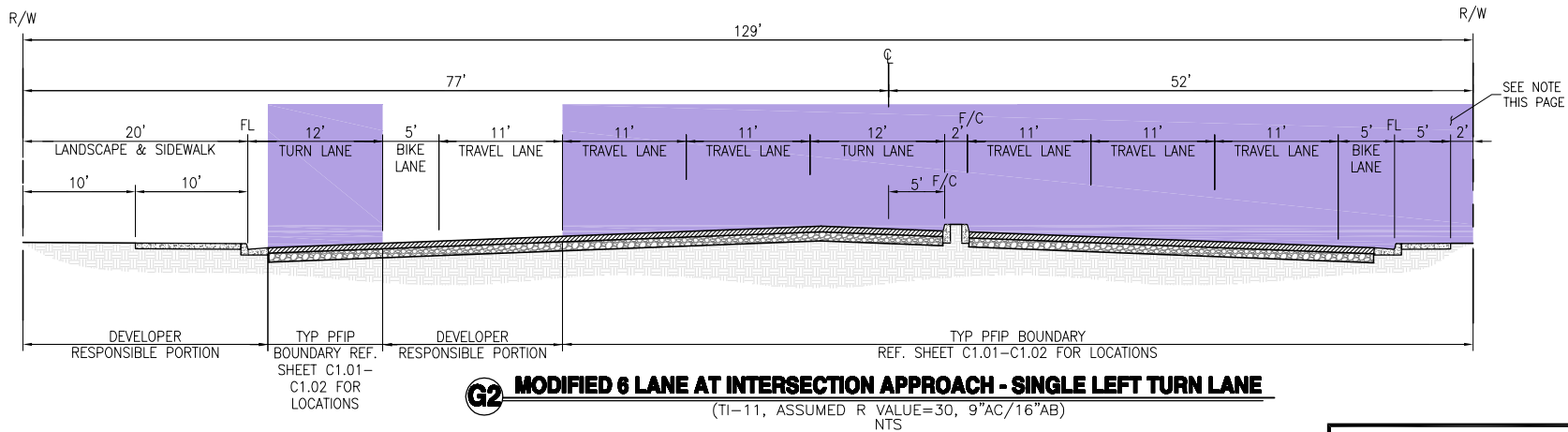
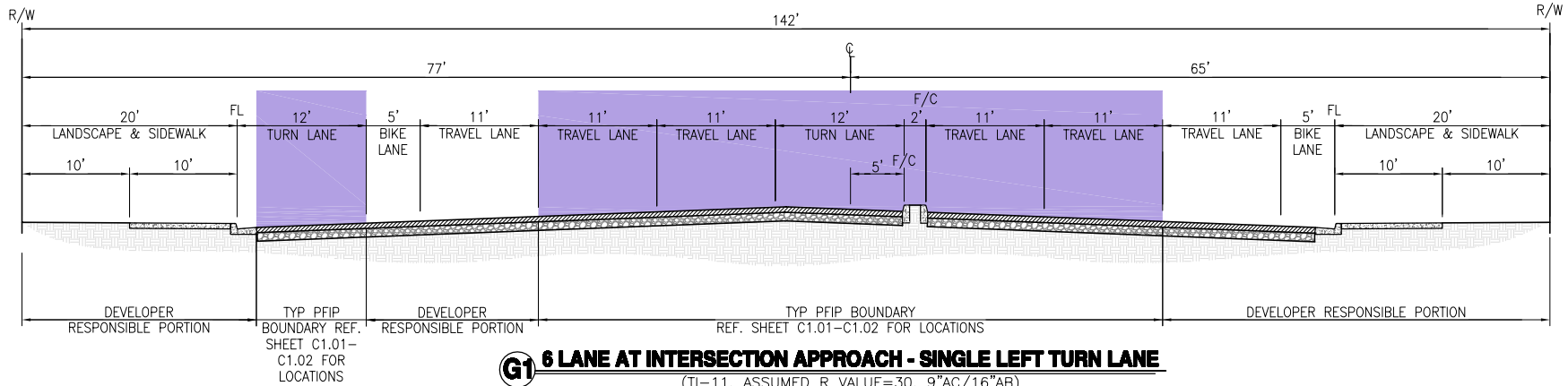
F4 MODIFIED AIRPORT WAY STREET SECTION, NORTH OF YOSEMITE
 (TI-11, ASSUMED R VALUE=30, 9"AC/16"AB)
 NTS

NOTE

THE PFIP LIMITS OF WORK ARE INCREASED IN AREAS WHERE FUTURE IMPROVEMENTS ARE NOT ANTICIPATED TO BE CONSTRUCTED AS PART OF FUTURE DEVELOPMENT PROJECTS. IN THESE SPECIFIC AREAS A REDUCED SIDEWALK AND PARKSTRIP IS TO BE UTILIZED AS SHOWN IN THIS CROSS SECTION. THESE ADDITIONAL IMPROVEMENTS MAY BE REQUIRED ON ONE SIDE OR BOTH SIDES OF THE SECTION TO COMPLETE THE PFIP IMPROVEMENTS. THESE AREAS ARE DENOTED ON THE PLANS. THE IMPROVEMENT COSTS ARE ACCOUNTED FOR IN THE PFIP FEE.

PLATE E-2.07

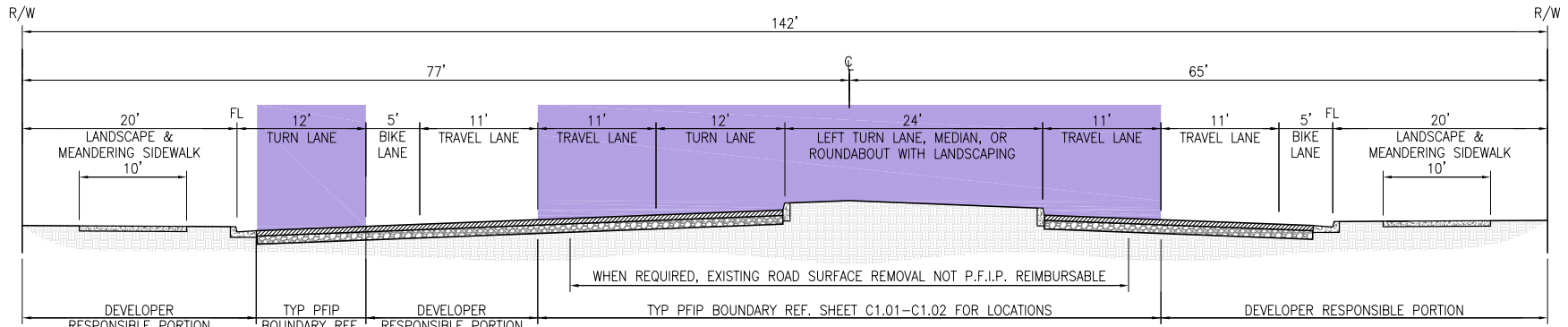
**AIRPORT WAY CROSS SECTION
 NORTH OF YOSEMITE**



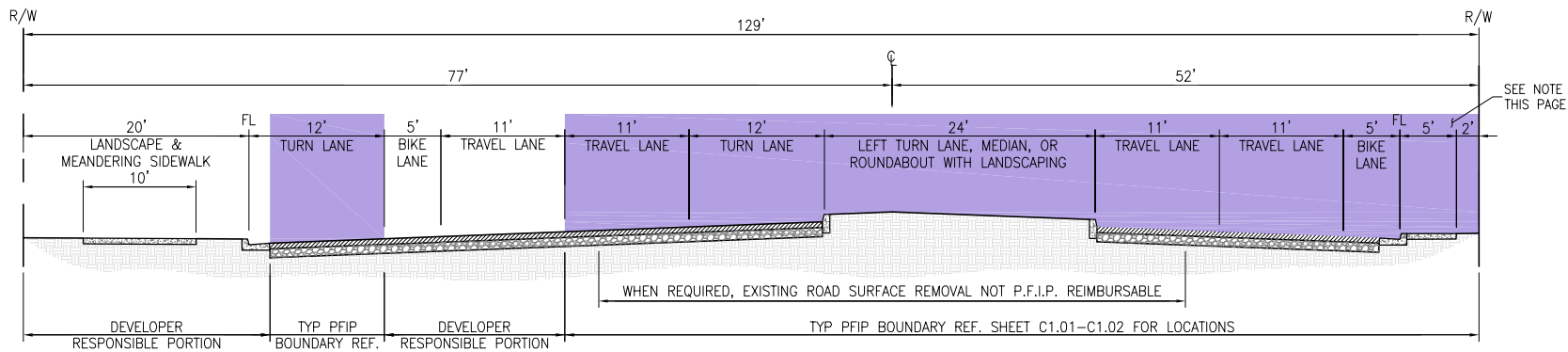
NOTE

THE PFIIP LIMITS OF WORK ARE INCREASED IN AREAS WHERE FUTURE IMPROVEMENTS ARE NOT ANTICIPATED TO BE CONSTRUCTED AS PART OF FUTURE DEVELOPMENT PROJECTS. IN THESE SPECIFIC AREAS A REDUCED SIDEWALK AND PARKSTRIP IS TO BE UTILIZED AS SHOWN IN THIS CROSS SECTION. THESE ADDITIONAL IMPROVEMENTS MAY BE REQUIRED ON ONE SIDE OR BOTH SIDES OF THE SECTION TO COMPLETE THE PFIIP IMPROVEMENTS. THESE AREAS ARE DENOTED ON THE PLANS. THE IMPROVEMENT COSTS ARE ACCOUNTED FOR IN THE PFIIP FEE.

PLATE E-2.08
6 LANE STREET CROSS-SECTIONS
AT INTERSECTIONS
SINGLE TURN LANE



G3 AIRPORT WAY STREET SECTION, NORTH OF YOSEMITE SINGLE TURN LEFT LANE
 (TI-11, ASSUMED R VALUE=20, 9"AC/16"AB)
 NTS

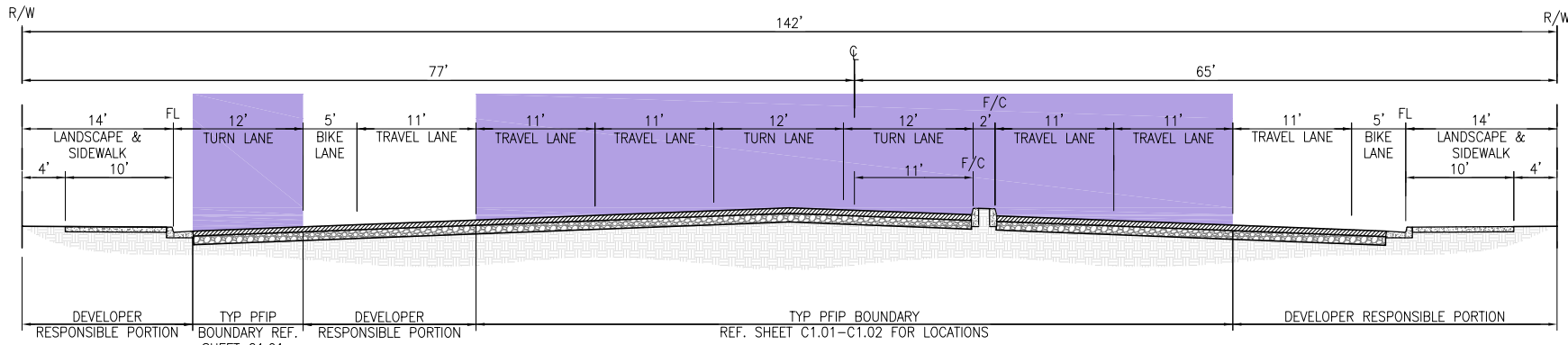
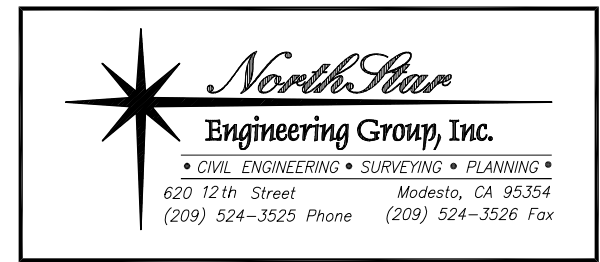


G4 MODIFIED AIRPORT WAY STREET SECTION, NORTH OF YOSEMITE SINGLE TURN LEFT LANE
 (TI-11, ASSUMED R VALUE=30, 9"AC/16"AB)
 NTS

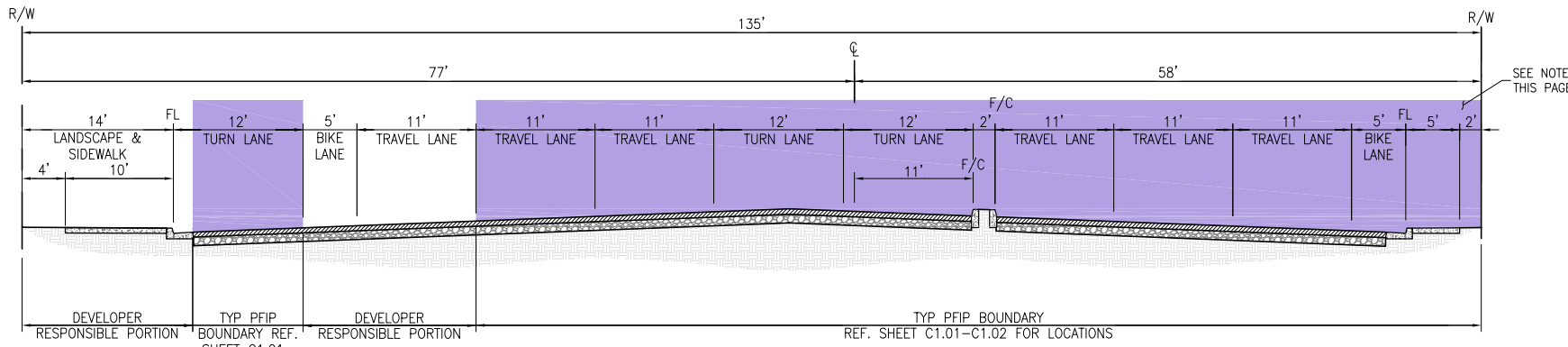
NOTE

THE PPIP LIMITS OF WORK ARE INCREASED IN AREAS WHERE FUTURE IMPROVEMENTS ARE NOT ANTICIPATED TO BE CONSTRUCTED AS PART OF FUTURE DEVELOPMENT PROJECTS. IN THESE SPECIFIC AREAS A REDUCED SIDEWALK AND PARKSTRIP IS TO BE UTILIZED AS SHOWN IN THIS CROSS SECTION. THESE ADDITIONAL IMPROVEMENTS MAY BE REQUIRED ON ONE SIDE OR BOTH SIDES OF THE SECTION TO COMPLETE THE PPIP IMPROVEMENTS. THESE AREAS ARE DENOTED ON THE PLANS. THE IMPROVEMENT COSTS ARE ACCOUNTED FOR IN THE PPIP FEE.

PLATE E-2.09
AIRPORT WAY CROSS SECTION AT INTERSECTIONS NORTH OF YOSEMITE SINGLE TURN LANE



H1 6 LANE AT INTERSECTION APPROACH - DOUBLE LEFT TURN LANE
 (TI-11, ASSUMED R VALUE=30, 9"AC/16"AB)
 NTS

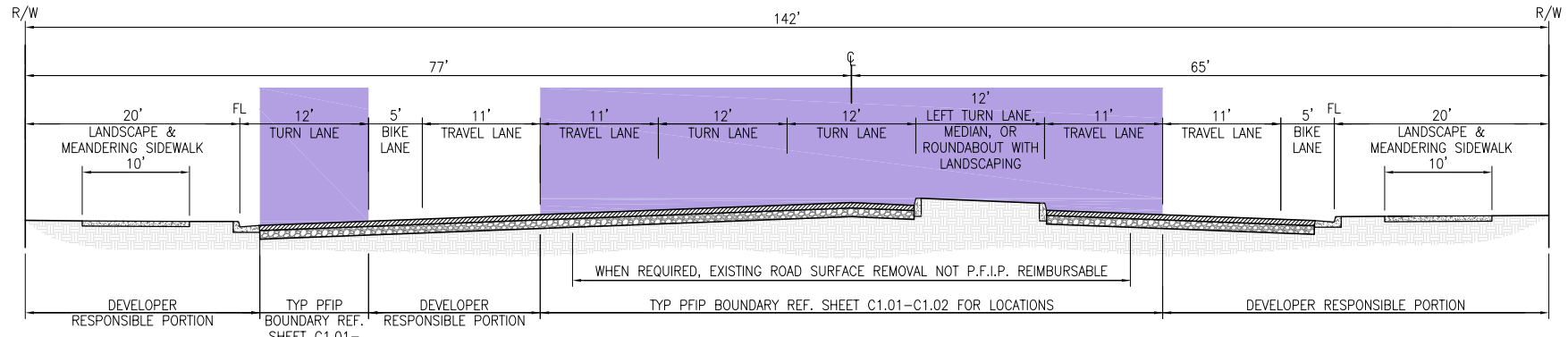


H2 MODIFIED 6 LANE AT INTERSECTION APPROACH - DOUBLE LEFT TURN LANE
 (TI-11, ASSUMED R VALUE=30, 9"AC/16"AB)
 NTS

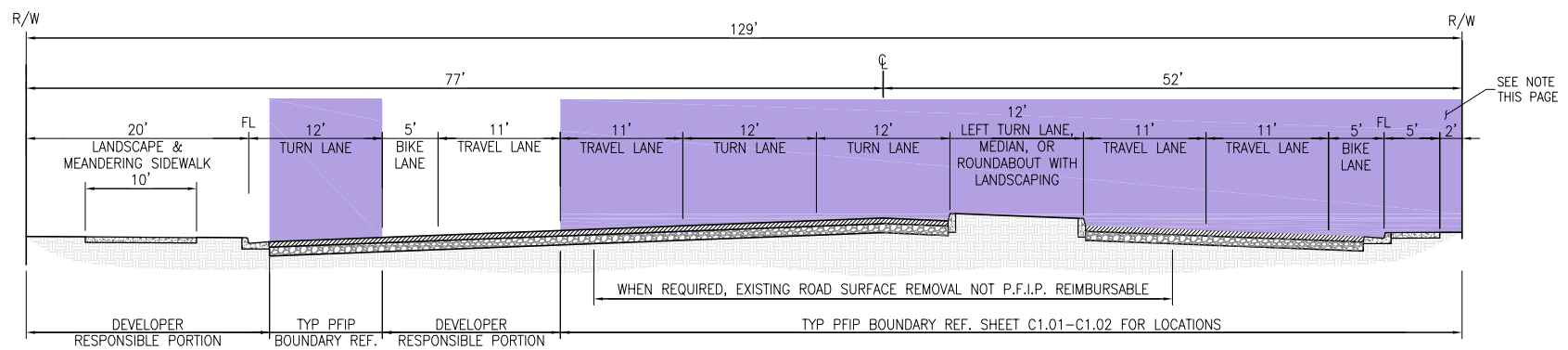
NOTE

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PLATE E-2.10
6 LANE STREET CROSS-SECTIONS
AT INTERSECTION
DOUBLE TURN LANE



H3 AIRPORT WAY STREET SECTION, NORTH OF YOSEMITE DOUBLE TURN LEFT LANE
 (TI-11, ASSUMED R VALUE=30, 9"AC/16"AB)
 NTS



H4 MODIFIED AIRPORT WAY STREET SECTION, NORTH OF YOSEMITE DOUBLE TURN LEFT LANE
 (TI-11, ASSUMED R VALUE=30, 9"AC/16"AB)
 NTS

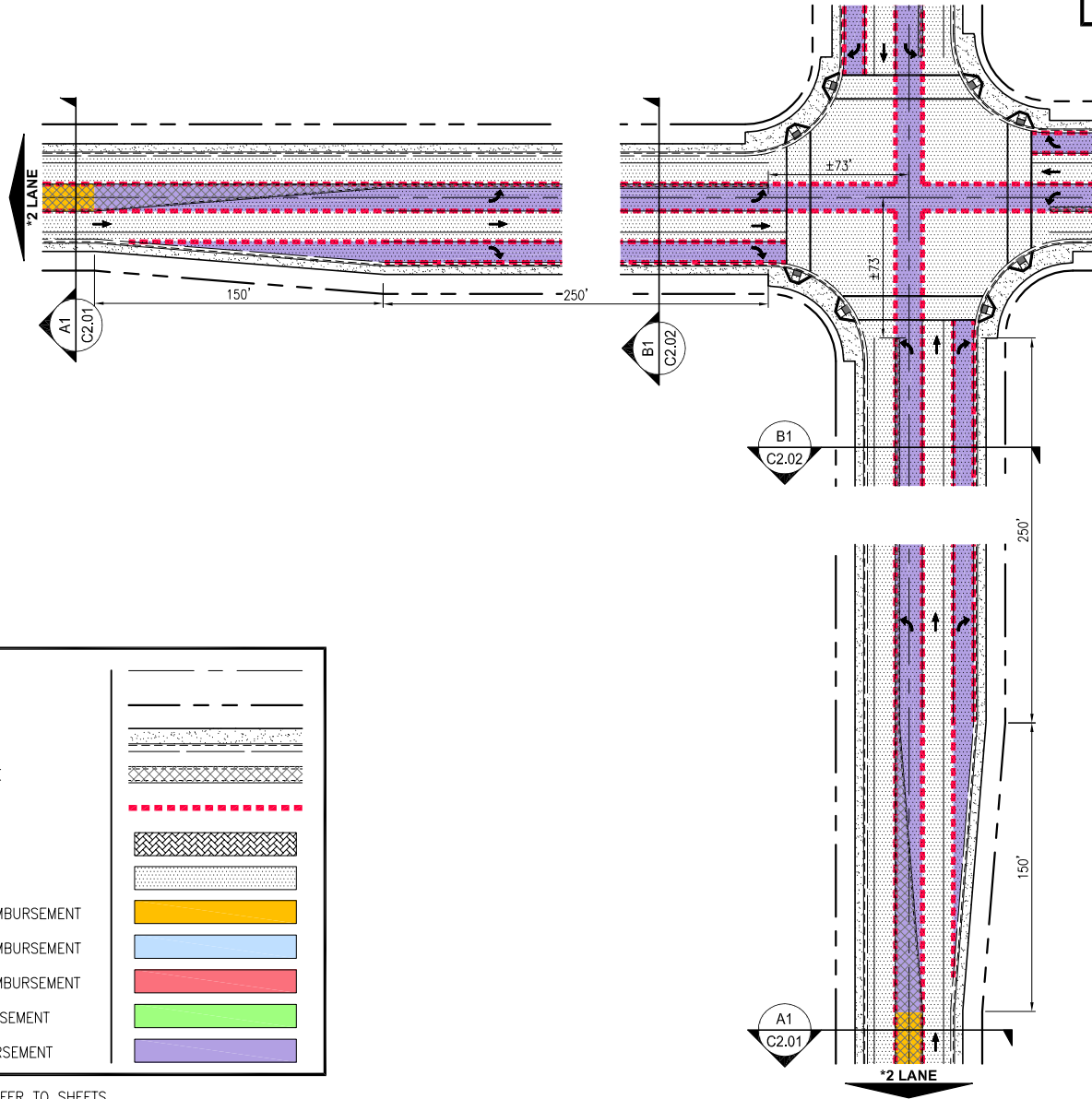
NOTE

THE PFIIP LIMITS OF WORK ARE INCREASED IN AREAS WHERE FUTURE IMPROVEMENTS ARE NOT ANTICIPATED TO BE CONSTRUCTED AS PART OF FUTURE DEVELOPMENT PROJECTS. IN THESE SPECIFIC AREAS A REDUCED SIDEWALK AND PARKSTRIP IS TO BE UTILIZED AS SHOWN IN THIS CROSS SECTION. THESE ADDITIONAL IMPROVEMENTS MAY BE REQUIRED ON ONE SIDE OR BOTH SIDES OF THE SECTION TO COMPLETE THE PFIIP IMPROVEMENTS. THESE AREAS ARE DENOTED ON THE PLANS. THE IMPROVEMENT COSTS ARE ACCOUNTED FOR IN THE PFIIP FEE.

PLATE E-2.11
AIRPORT WAY CROSS SECTION
NORTH OF YOSEMITE
DOUBLE TURN LANE



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LEGEND

CENTERLINE	
RIGHT-OF-WAY	
CURB, GUTTER AND SIDEWALK	
MEDIAN CURB AND LANDSCAPE	
P.F.I.P. BOUNDARY	
TRUCK APRON	
PAVEMENT	
2 LANE ROADWAY P.F.I.P. REIMBURSEMENT	
4 LANE ROADWAY P.F.I.P. REIMBURSEMENT	
6 LANE ROADWAY P.F.I.P. REIMBURSEMENT	
ROUNDBOUT P.F.I.P. REIMBURSEMENT	
INTERSECTION P.F.I.P. REIMBURSEMENT	

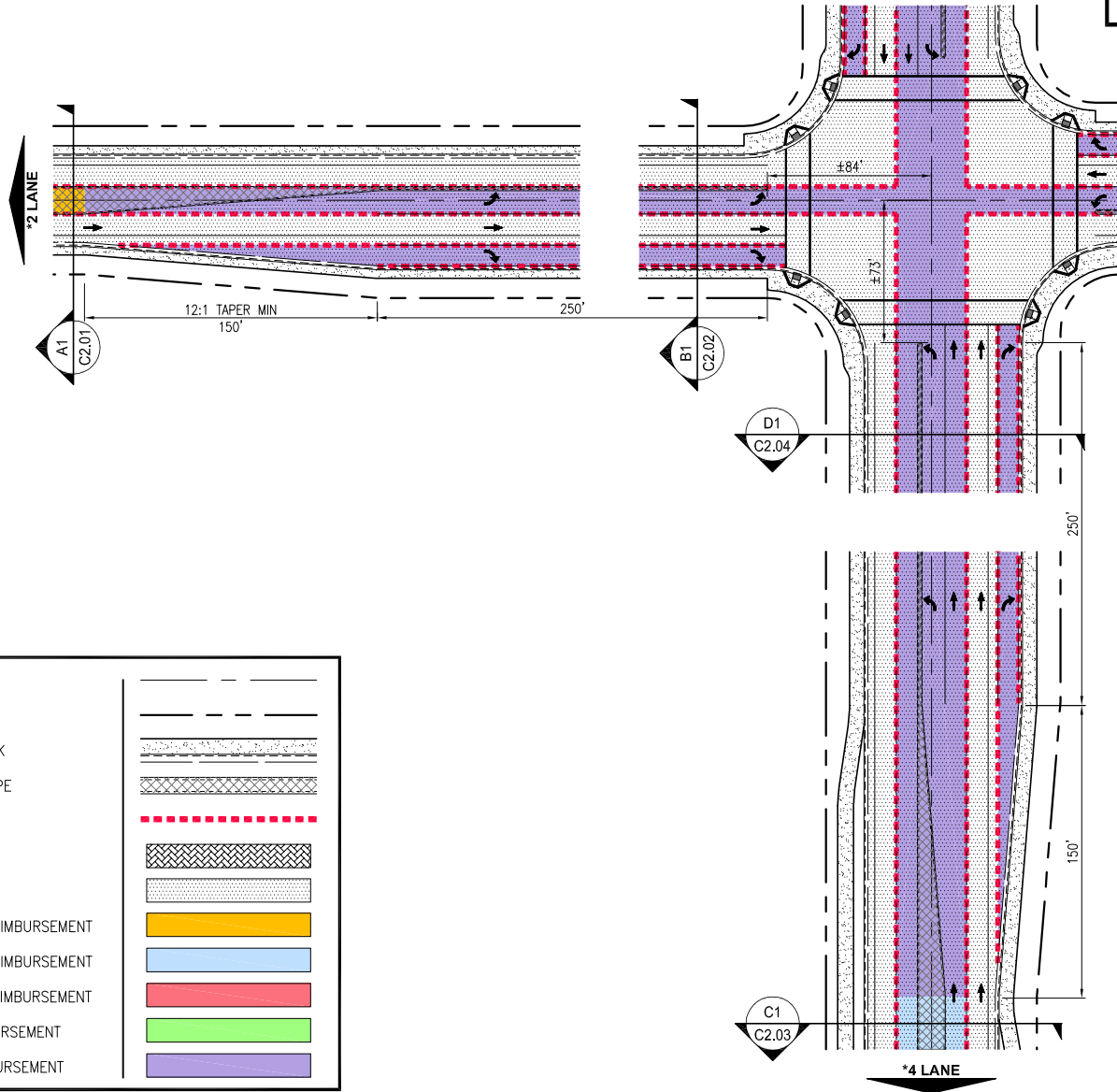
*FOR TYPICAL X-SECTIONS REFER TO SHEETS
 C2.01-C2.11 FOR ADDITIONAL INFORMATION

PLATE E-3.01

**2X2 TYPICAL INTERSECTION
 SINGLE TURN LANE**



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CURB, GUTTER AND SIDEWALK	
MEDIAN CURB AND LANDSCAPE	
P.F.I.P. BOUNDARY	
TRUCK APRON	
PAVEMENT	
2 LANE ROADWAY P.F.I.P. REIMBURSEMENT	
4 LANE ROADWAY P.F.I.P. REIMBURSEMENT	
6 LANE ROADWAY P.F.I.P. REIMBURSEMENT	
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INTERSECTION P.F.I.P. REIMBURSEMENT	

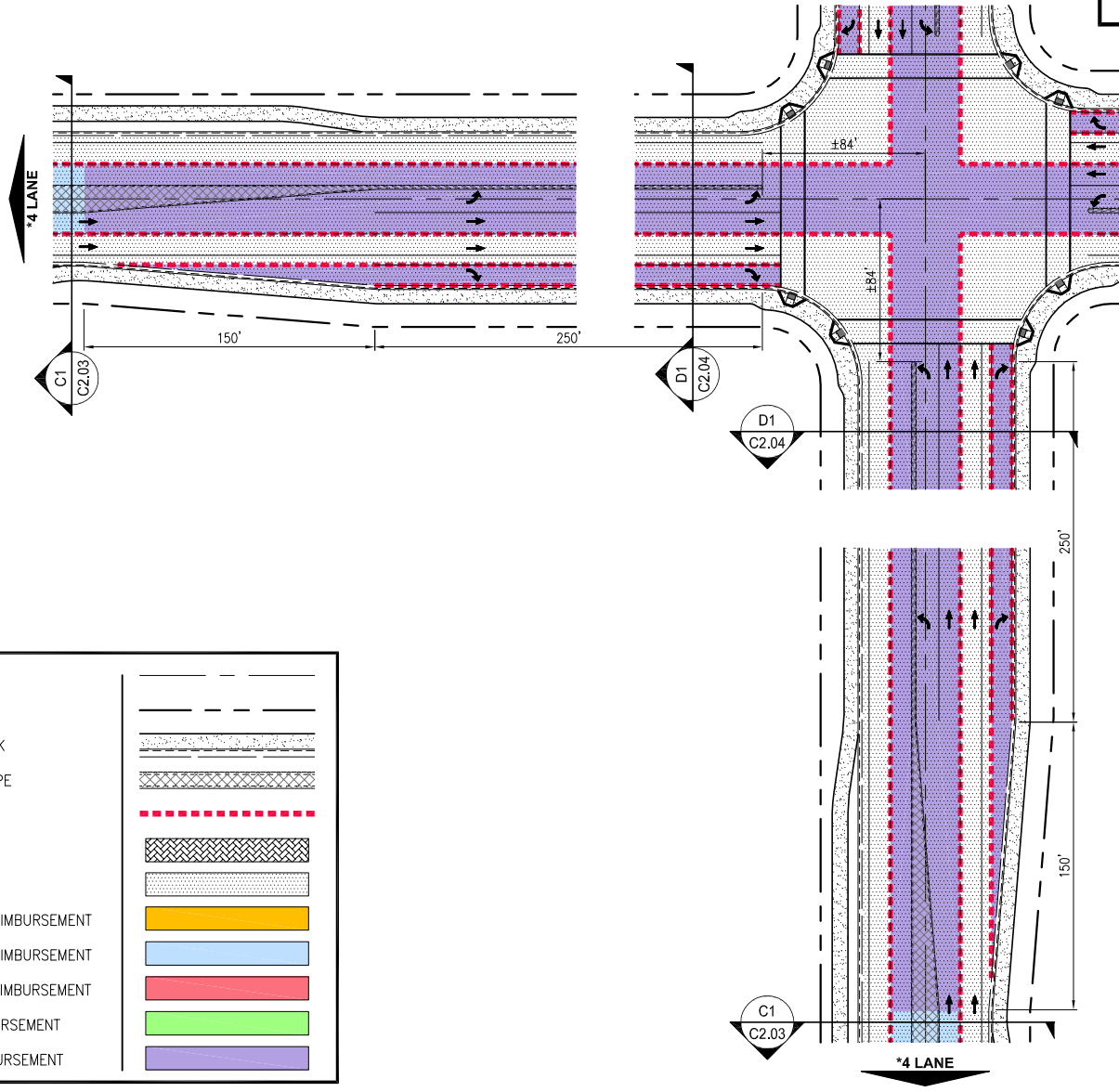
*FOR TYPICAL X-SECTIONS REFER TO SHEETS
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PLATE E-3.02

**2 TO 4 TYPICAL INTERSECTION
 SINGLE TURN LANE**



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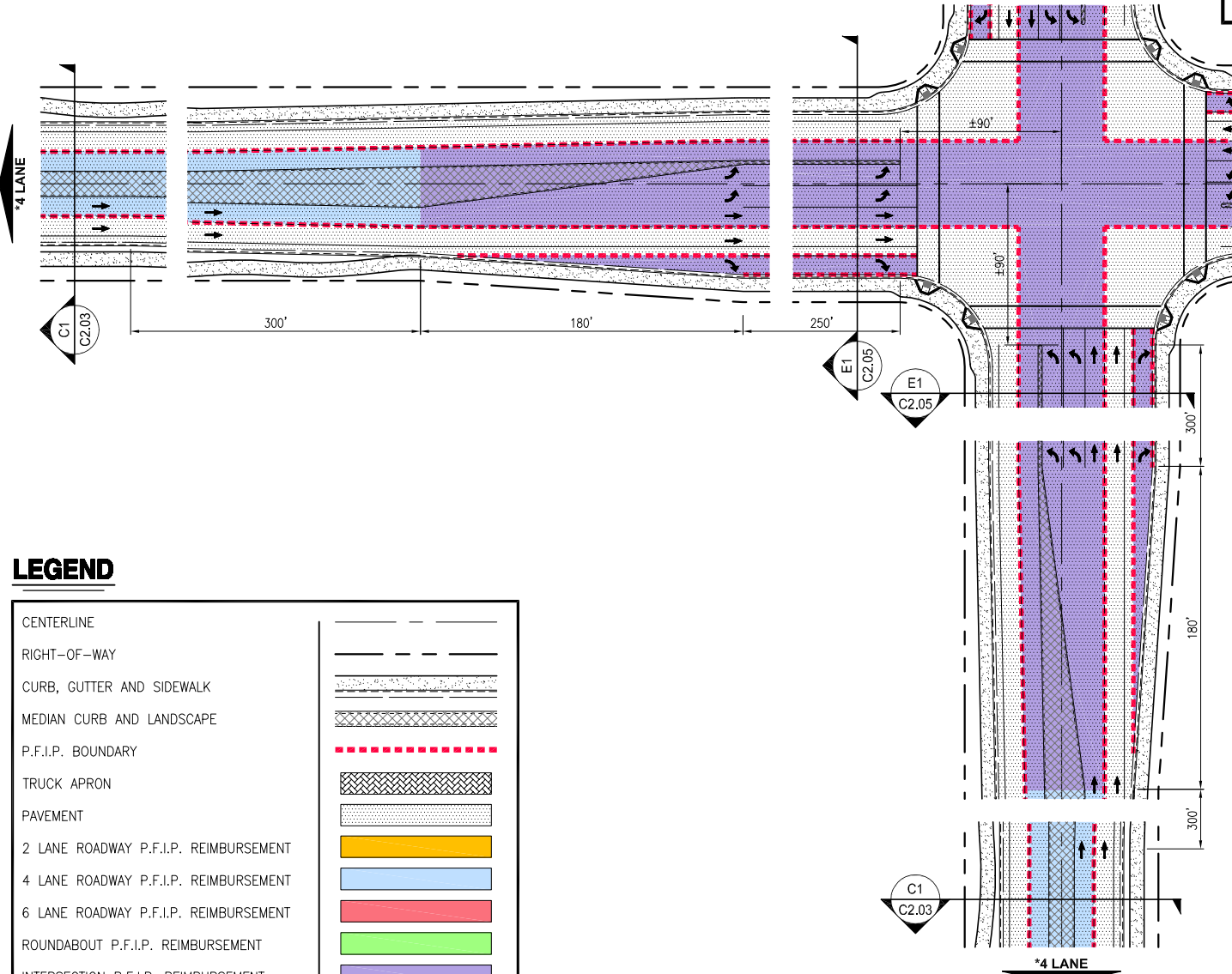
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TRUCK APRON	
PAVEMENT	
2 LANE ROADWAY P.F.I.P. REIMBURSEMENT	
4 LANE ROADWAY P.F.I.P. REIMBURSEMENT	
6 LANE ROADWAY P.F.I.P. REIMBURSEMENT	
ROUNDBOUT P.F.I.P. REIMBURSEMENT	
INTERSECTION P.F.I.P. REIMBURSEMENT	

*FOR TYPICAL X-SECTIONS REFER TO SHEETS
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PLATE E-3.03
**4-LANE TYPICAL ARTERIAL
 SINGLE TURN LANE**



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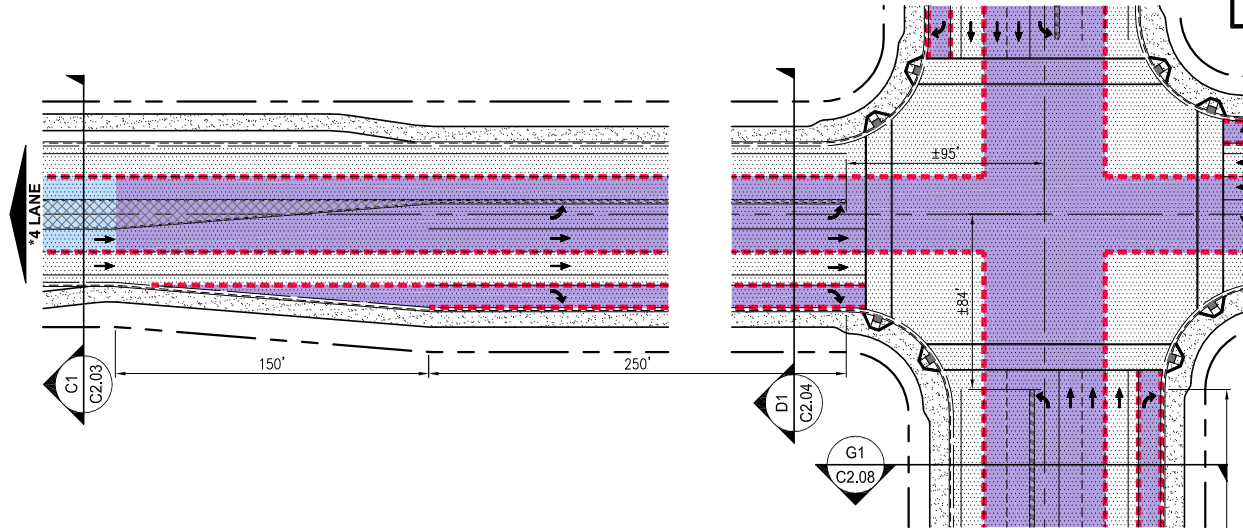
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TRUCK APRON	
PAVEMENT	
2 LANE ROADWAY P.F.I.P. REIMBURSEMENT	
4 LANE ROADWAY P.F.I.P. REIMBURSEMENT	
6 LANE ROADWAY P.F.I.P. REIMBURSEMENT	
ROUNDBOUNT P.F.I.P. REIMBURSEMENT	
INTERSECTION P.F.I.P. REIMBURSEMENT	

*FOR TYPICAL X-SECTIONS REFER TO SHEETS
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PLATE E-3.04
**4-LANE TYPICAL ARTERIAL
 DOUBLE TURN LANE**



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RIGHT-OF-WAY	
CURB, GUTTER AND SIDEWALK	
MEDIAN CURB AND LANDSCAPE	
P.F.I.P. BOUNDARY	
TRUCK APRON	
PAVEMENT	
2 LANE ROADWAY P.F.I.P. REIMBURSEMENT	
4 LANE ROADWAY P.F.I.P. REIMBURSEMENT	
6 LANE ROADWAY P.F.I.P. REIMBURSEMENT	
ROUNDBOUT P.F.I.P. REIMBURSEMENT	
INTERSECTION P.F.I.P. REIMBURSEMENT	

*FOR TYPICAL X-SECTIONS REFER TO SHEETS
 C2.01-C2.11 FOR ADDITIONAL INFORMATION

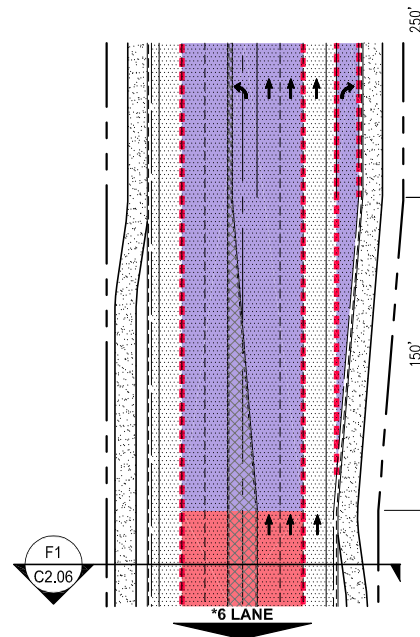
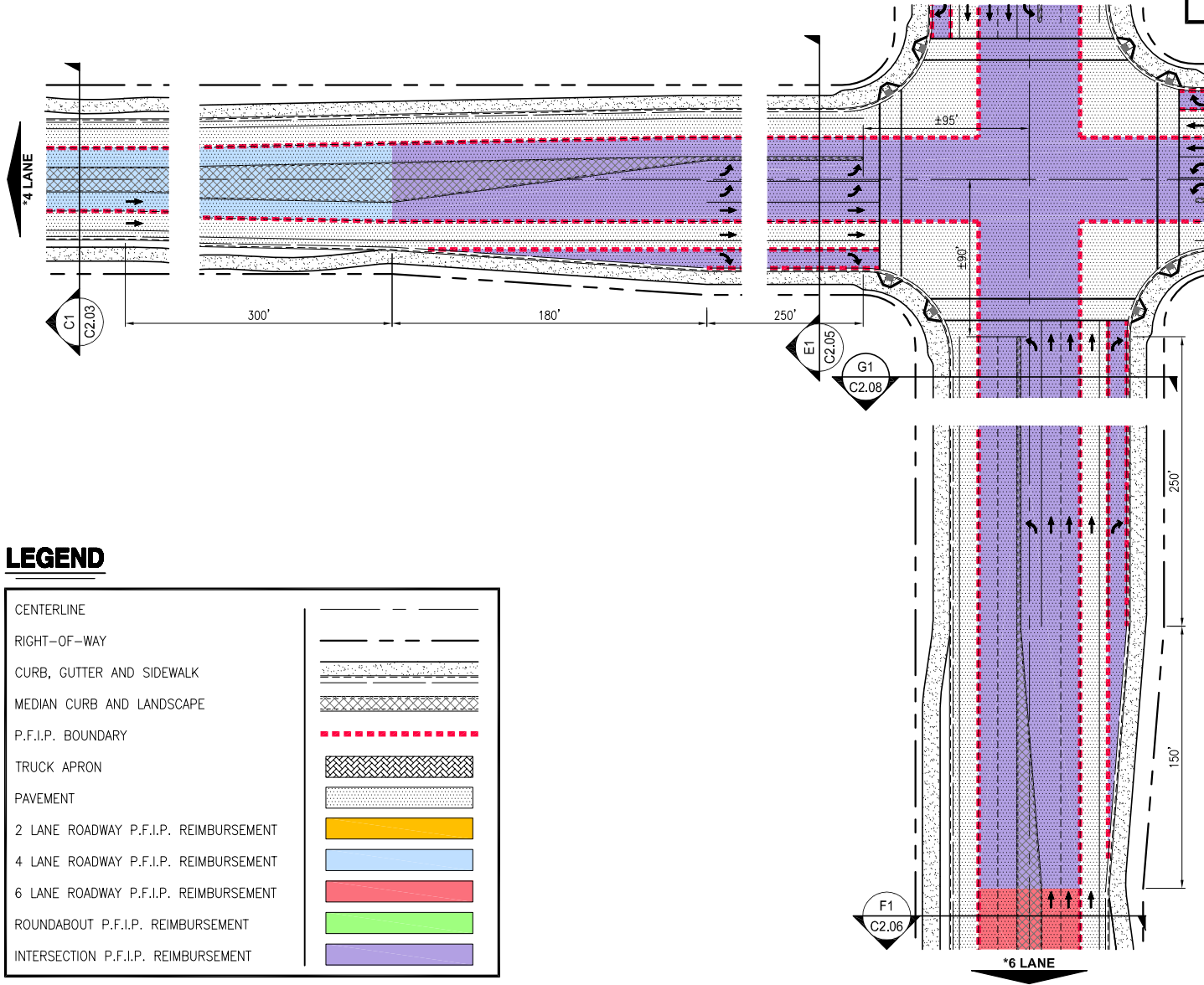


PLATE E-3.05

**4 X 6 TYPICAL INTERSECTION
 SINGLE TURN LANE**



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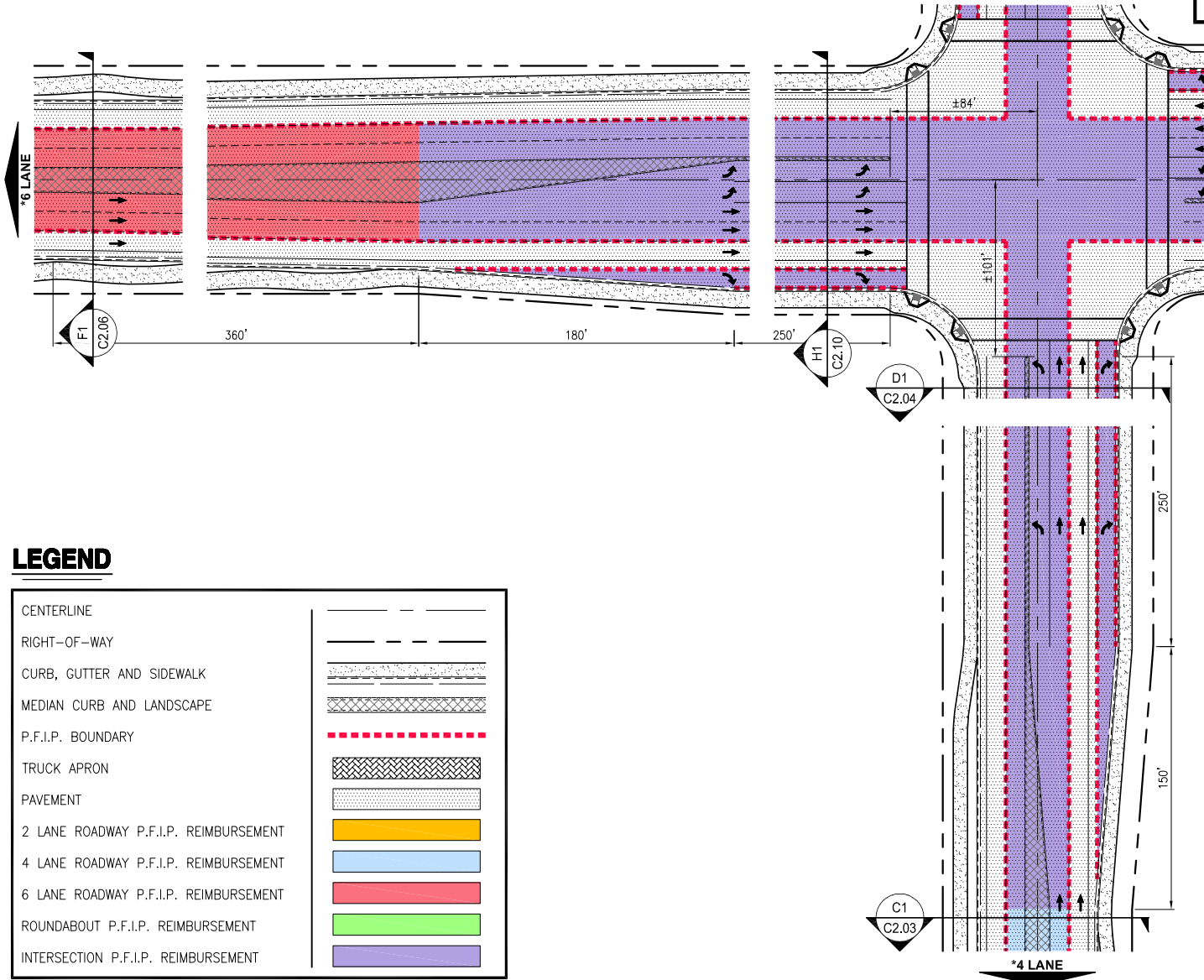
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CURB, GUTTER AND SIDEWALK	
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TRUCK APRON	
PAVEMENT	
2 LANE ROADWAY P.F.I.P. REIMBURSEMENT	
4 LANE ROADWAY P.F.I.P. REIMBURSEMENT	
6 LANE ROADWAY P.F.I.P. REIMBURSEMENT	
ROUNDBOUNT P.F.I.P. REIMBURSEMENT	
INTERSECTION P.F.I.P. REIMBURSEMENT	

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PLATE E-3.06
4 TO 6 LANE TYPICAL ARTERIAL DOUBLE TURN LANE



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CENTERLINE	
RIGHT-OF-WAY	
CURB, GUTTER AND SIDEWALK	
MEDIAN CURB AND LANDSCAPE	
P.F.I.P. BOUNDARY	
TRUCK APRON	
PAVEMENT	
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4 LANE ROADWAY P.F.I.P. REIMBURSEMENT	
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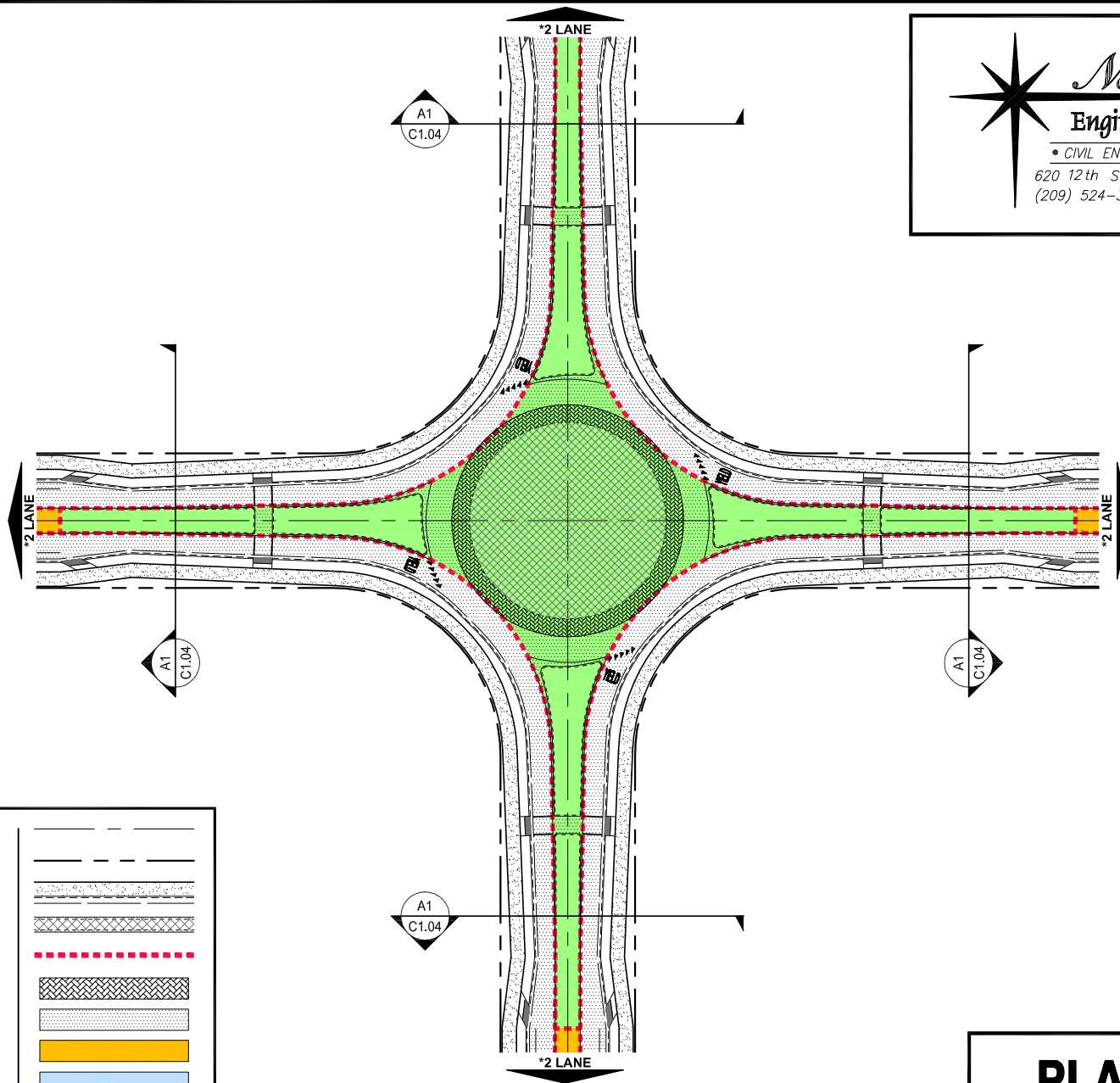
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PLATE E-3.07

**6 TO 4 LANE TYPICAL ARTERIAL
 DOUBLE TURN LANE**



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CENTERLINE	
RIGHT-OF-WAY	
CURB, GUTTER AND SIDEWALK	
MEDIAN CURB AND LANDSCAPE	
P.F.I.P. BOUNDARY	
TRUCK APRON	
PAVEMENT	
2 LANE ROADWAY P.F.I.P. REIMBURSEMENT	
4 LANE ROADWAY P.F.I.P. REIMBURSEMENT	
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ROUNDBOUT P.F.I.P. REIMBURSEMENT	
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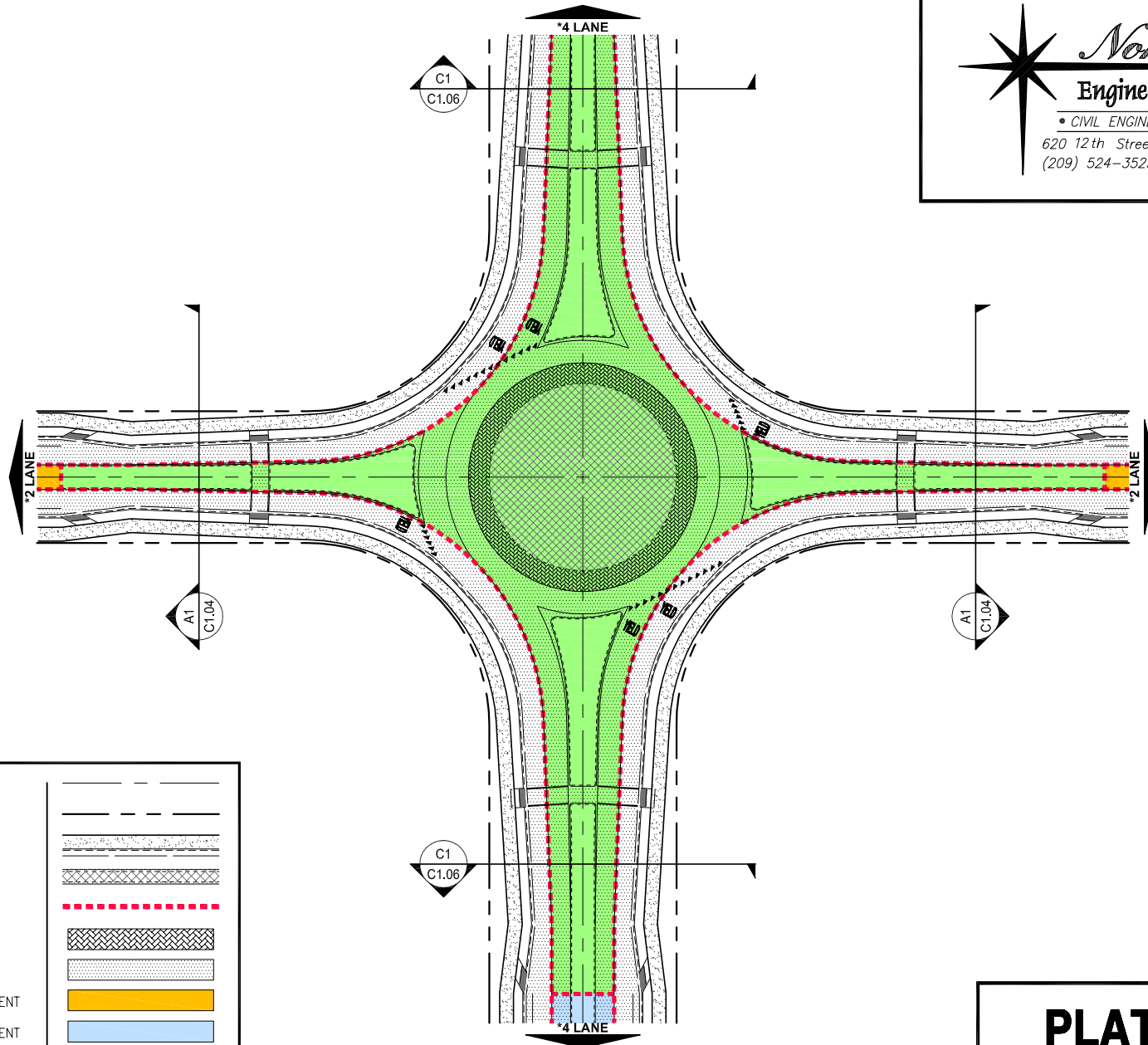
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PLATE E-3.08

**2 TO 2 LANE TYPICAL ARTERIAL
 ROUNDABOUT**



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CENTERLINE	
RIGHT-OF-WAY	
CURB, GUTTER AND SIDEWALK	
MEDIAN CURB AND LANDSCAPE	
P.F.I.P. BOUNDARY	
TRUCK APRON	
PAVEMENT	
2 LANE ROADWAY P.F.I.P. REIMBURSEMENT	
4 LANE ROADWAY P.F.I.P. REIMBURSEMENT	
6 LANE ROADWAY P.F.I.P. REIMBURSEMENT	
ROUNDBOUT P.F.I.P. REIMBURSEMENT	
INTERSECTION P.F.I.P. REIMBURSEMENT	

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PLATE E-3.09
**2 TO 4 LANE TYPICAL ARTERIAL
 ROUNDABOUT**